

ANNING APPLICATIONS COMMITTEE
18th August 2022

Item No:

UPRN

APPLICATION NO.

DATE VALID

21/P0400

20/01/2021

Address/Site:

19 Worples Road, Wimbledon, SW19 4JS

(Ward)

Hillside

Proposal:

Demolition of existing structures & redevelopment to provide a new 8-storey (plus basement) building comprising 7330sqm of GIA floorspace, with retail at ground and first floor levels and office above (Class E); landscaping; ground works; associated infrastructure and development

Drawing Nos:

ZZ-XX-DR-A-91-0001(P10), ZZ-XX-DR-A-91-0002(P4),
ZZ-01-DR-A-01-0001(P16), ZZ-GF-DR-A-01-0001(P18),
ZZ-01-DR-A-01-0001(P16), ZZ-02-DR-A-01-0001(P12),
ZZ-03-DR-A-01-0001(P12), ZZ-04-DR-A-01-0001(P12),
ZZ-05-DR-A-01-0001(P12), ZZ-06-DR-A-01-0001(P12),
ZZ-07-DR-A-01-0001(P12), ZZ-RF-DR-A-01-0001(P7),
ZZ-XX-DR-A-02-0001(P10), ZZ-XX-DR-A-02-0002(P9),
ZZ-XX-DR-A-02-0003(P9), ZZ-XX-DR-A-02-0004(P10),
ZZ-XX-DR-A-03-0001(P9), ZZ-XX-DR-A-03-0002(P8),
ZZ-XX-DR-A-03-0003(P8), ZZ-XX-DR-A-03-0004(P9),
ZZ-XX-DR-A-03-0005(P9), 892-PL-002(P02)

Contact Officer:

David Gardener (0208 545 3115)

RECOMMENDATION

GRANT Planning Permission Subject to completion of a S106 Agreement, and conditions

CHECKLIST INFORMATION

- Heads of agreement: S278 for public realm improvements, carbon offset, financial contribution for short stay cycle spaces, financial contribution for on-street disabled parking bay, restriction on size of servicing and delivery vehicles
- Is a screening opinion required: No
- Is an Environmental Statement required: No
- Has an Environmental Impact Assessment been submitted: No

- Press notice: Yes
- Site notice: Yes
- Design Review Panel consulted: No
- Number of neighbours consulted: 129
- External consultations: Transport for London, Metropolitan Police (Designing out crime), Thames Water, Historic England

1. INTRODUCTION

- 1.1 The application has been brought before the Planning Applications Committee due to the nature and number of objections received following public consultation.

2. SITE AND SURROUNDINGS

- 2.1 The application site comprises a 1960s built seven storey Use Class E(g)(i) office building with 5,390sqm gross internal floorspace (GIA), which is located on the north side of Worple Road.
- 2.2 The immediate surrounding area is characterised by buildings of varying heights with building ranging between two and eight storeys in height. The site is bounded by low rise residential buildings to the north, Wimbledon Central, a seven storey residential building, to the southwest, and Swan Court, a five storey office building to the Northeast. Buildings on the opposite side of Worple Road are commercial, comprising the department store, Ely's and food retailer, Sainsbury's.
- 2.3 The application site is not subject to any statutory heritage asset designations although the site sits south and west of the Merton (Wimbledon Hill Road) Conservation Area.
- 2.4 The application site currently comprises 68 off-street car parking spaces, which are accessed from Worple Road. The application site has excellent public transport links (PTAL rating of 6b) being sited in very close proximity to Wimbledon tube, railway and tram station and a number of bus routes. The site is also located in a Controlled Parking Zone (Zone W1), and is subject to a range of parking controls including pay & display bays, blue badge parking bays and taxi only bays. At the site frontage there are three parking bays which allow for a maximum of two hours of parking during the hours of control, comprising 0930-1830 Monday to Saturday and 1000-1600 on Sundays. Servicing from these bays is prohibited between 0830-0930 Monday to Saturday.

3. CURRENT PROPOSAL

- 3.1 The proposal is to demolish the existing building and erect a replacement eight storey building (plus basement) with 7330sqm of GIA floorspace. This would provide an uplift of 1940sqm of floorspace compared to existing. The proposal would comprise 2108sqm of retail (Use Class E(a)) at ground and first floor level, and 5069sqm of office (Use Class E(g)(i)) above. An additional 153sqm,

which comprises some plant and basement access areas, would be shared space. The proposed building would have a maximum height of 29.8m

- 3.2 Facing materials would comprise buff blend brick on the bulk of the front, rear and side elevations with a secondary material of zinc (with red/copper tone) featured on the suspended block over the vehicle access and southwest facing side elevations. Curtain wall glazing would feature at ground and first floor levels on the front elevation, with grey RAL 7024 framing of the curtain wall shopfront. Terraces would be located at level 2 on the southwest side of the building, level 6 on the northeast side, and level 7 at the front.
- 3.3 No off-street car parking is proposed with the existing car park egress from Worple Road retained to enable access to the sub-station at the rear of the site. Servicing & refuse collection will take place on-street. This would involve shared use of the three parking bays directly in front of the site (on the west side of the existing site access) with loading only would be permitted between the hours of 7:00am-8:30am and 7:00pm-9:00pm (Mondays to Saturdays). Between the hours 8:30am-7:00pm (Mondays to Saturdays) and 10:00am-4:00pm (Sundays) the bays would be available for Pay and Display parking only (i.e. no loading), whilst overnight no restrictions would apply.
- 3.4 A total of 92 Long Stay Cycle spaces would be provided at basement level, whilst 16 short stay cycle spaces would be provided outside the front of the building. Shower and changing facilities will be provided at basement level.
- 3.5 The applicant has submitted a proposed landscape strategy, which proposes re-paving the outside the Worple Road frontage of the building in Yorkstone paving. Raised planters and external furniture is proposed for the external terraces.
- 3.6 Amended Plans: Please note that the application has been amended since it was first submitted. The glazing above the office entrance has been removed and replaced by an additional column of buff brickwork, which now wraps around the building corner. The rear of the building has been reduced in height through the reduction in depth of the top two floors. The rear of the building has also been reduced in depth by 2m. The proposed public realm has also been amended with the removal of the proposed paving bands with Yorkstone paving now proposed throughout. The proposed servicing and delivery arrangement has been amended with on-street servicing now proposed instead of off-street. This will incorporate the use of the existing 3 on-street parking bays outside the front of the building.

4. PLANNING HISTORY

The following planning history is relevant:

- 4.1 MER101/85 - Redevelopment of site by the erection of a part 5 storey part 7 storey office building comprising 4770 sqm gross with car parking involving demolition of existing building on site. Refused – 18/04/1985, for the following reason:

“ By reason of its height and massing, the proposed office building would have an adverse effect on the amenities of occupiers of adjoining residential properties in Mansel Road.”

- 4.2 98/P0454 - Recladding of existing elevations of building (excluding ground floor retail unit). Granted - 18/06/1998
- 4.3 99/P1192 - Erection of a part single, part two storey extension at the rear of the existing building (586 sqm) (outline planning permission). Granted - 13/04/2000
- 4.4 03/P2555 - Extension at the rear of the existing building to increase the height from five storeys to seven storeys to the same height as the front of the building. (outline planning permission). Granted - 15/01/2004
- 4.5 03/P2567 - Extension at the rear of the existing building to increase from five storeys to seven storeys to the same height as the front of the building (Application for approval of reserved matters following grant of outline planning permission on appeal in November 2000 (Ref: 99/P1191).
- 4.6 In September 2019 pre-application advice was sought for the demolition of existing building and erection of a nine storey building comprising office floorspace and a new retail premises at ground floor only (LBM Ref: 19/P3442)

5. POLICY CONTEXT

- 5.1 The following policies from the Adopted Sites and Policies Plan and Policies Maps (July 2014):
DM D1 (Urban design and public realm), DM D2 (Design considerations in all developments), DM D3 (Alterations and extensions to existing buildings), DM D4 (Managing heritage assets), DM E1 (Employment areas in Merton), DM E2 (Offices in town centres), DM EP2 (Reducing and mitigating noise), DM F1 (Support for flood risk management), DM F2 (Sustainable urban drainage systems SuDS, wastewater and water infrastructure) DM R1 (Location and scale of development in Merton’s town centres and neighbourhood parades), DM T1 (Support for sustainable transport and active travel), DM T2 (Transport Impacts of development), DM T3 (Car parking and servicing standards), DM T5 (Access to the road network)
- 5.2 The relevant policies in the Adopted Core Strategy (July 2011) are:
CS.6 (Wimbledon Town Centre), CS.7 (Centres), CS.12 (Economic development), CS.14 (Design), CS.15 (Climate Change), CS. 16 (Flood Risk Management), CS.18 (Active Transport), CS.19 (Public Transport), CS.20 (Parking, Servicing and Delivery)
- 5.3 The relevant policies in the London Plan (2021) are:
GG5 (Growing a good economy), GG6 (Increasing efficiency and resilience), SD6 (Town centres and High Streets), SD8 (Town Centre Network), D2 (Infrastructure requirements for sustainable densities), D3 (Optimising site capacity through the design-led approach), D4 (Delivering Good Design), D5

(Inclusive design), D8 (Public Realm), D9 (Tall Buildings), D11 (Safety, security and resilience to emergency), D12 (Fire safety), E1 (Offices), E2 (Providing suitable business space), E3 (Affordable Workspace), E9 (Retail, markets and hot food takeaways), HC1 (Heritage conservation and growth), SI 1 (Improving air quality), SI 2 (Minimising greenhouse gas emissions), SI 3 (Energy infrastructure), SI 4 (Managing heat risk), SI 5 (Water infrastructure), Policy SI 7 (Reducing waste and supporting the circular economy), SI 13 (Sustainable drainage), T4 (Assessing and mitigating transport impacts), T5 (Cycling), T6 (Car parking), T6.2 (Office parking), T6.3 (Retail parking), T6.5 (Non-residential disabled persons parking), T7 (Deliveries, servicing and construction)

5.4 National Planning Policy Framework 2021 (NPPF)

5.5 National Planning Practice Guidance 2014 (NPPG)

5.6 Future Wimbledon Supplementary Planning Document (SPD) (November 2020)

5.7 Merton's Tall Buildings Background Paper 2010

6. CONSULTATION

6.1 The application was originally publicised by means of a site notice and individual letters to occupiers of neighbouring properties. In response, 40 letters of objection were received. The letters of objection, which include objections from The Wimbledon Society, Wimbledon E Hillside Residents' Association (WEHRA), Raymond and Mansel Road Residents Association, Wimbledon Central Residents Association Ltd, and Friends of Wimbledon Town Centre, are on the following grounds:

- Loss of daylight/sunlight, inaccuracy of daylight/sunlight report/rights of light impact/No overshadowing report
- Loss of privacy/overlooking
- Excessive height/fails to relate to the height and massing of surrounding buildings/overbearing impact/visually intrusive
- Overdevelopment of site
- Existing tree located behind the site would not screen proposal/impact on conservation area
- Overconcentration of supermarkets in the locality
- Disruption due to noise, dust and debris and inconvenience to pedestrians during construction
- Lack of parking for shopping
- Proposed construction vehicles are of insufficient size to remove required debris during demolition phase
- Congestion from retail shoppers using on-street parking bays and disruption this will cause to pedestrian movement and bus stops. Increased parking pressure on surrounding residential streets
- Impact on safety
- Air quality/odour/noise

- Flood risk
- Employment numbers stated are not a NET gain
Adverse impact on the variety, vitality & viability of retail in Wimbledon's Primary Shopping Area, and other local centres
- Lack of prior notification
- Light pollution
- Lack of sustainability due to demolition of a modern building/no attempt to upgrade existing building/lack of future proofing/increase carbon emissions

6.2 Wimbledon Society

Object to the proposal due to lack of sustainability, excessive height, which does not relate to the height and massing of surrounding buildings, and loss of daylight/sunlight and privacy to neighbouring properties. Also object to the proposed servicing on safety grounds due to heavy goods vehicles entering and exiting the site from Worple Road.

6.3 Wimbledon E Hillside Residents' Association (WEHRA)

Object to the proposal on grounds of sustainability, flood risk, excessive height, moving forward of building line which will impact ability to 'green' the local area and make the pavement area more congested. Also object to the proposed servicing on safety grounds due to heavy goods vehicles entering and exiting the site from Worple Road, and lack of parking for staff or shoppers due to increased congestion and the pressure it will put on parking spaces in the surrounding road network.

6.4 Raymond and Mansel Road Residents Association (RAMRA)

Object to the proposal. The proposal is excessive in terms of height and bulk and would have a detrimental impact on properties located at the rear and at Wimbledon central, including loss of daylight/sunlight, overshadowing of rear gardens, loss of privacy, noise, and visual intrusion. Also object on grounds of lack of sustainability, flood risk and pollution.

6.5 Friends of Wimbledon Town Centre

Object to the proposal on the grounds of loss of daylight/sunlight, privacy, visual intrusion, excessive height, failure to respect the adjacent conservation area, and sustainability. The proposed building also does not relate to surrounding buildings, would have a negative impact on parking locally, and the proposed increase in heavy goods and customer vehicles will have a negative impact on highway safety.

6.6 Wimbledon Central Residents Association Ltd

Object to the proposal concerning loss of daylight/sunlight, rights to light infringement, loss of privacy, sustainability, and unnecessary demolition. There was also lack of any consultation prior to the submission of the application, which has resulted in significant inaccuracies in the application.

6.7 Following amendments to the proposal a further re-consultation was carried out. In response, a further 17 letters of objection, including a letter of objection from the Wimbledon Central Residents Association Ltd were received

reiterating a number of the concerns raised during the previous round of consultation.

6.8 Transport for London

The site of the proposed development is approximately 1km from the A238 Kingston Road which forms part of the Strategic Road Network (SRN). TfL has a duty under the Traffic Management Act 2004 to ensure that any development does not have an adverse impact on the SRN.

6.9 TfL supports the car-free nature of the proposal. It is noted that no Blue Badge (BB) parking is to be provided as part of the proposed development. It is recommended that a contribution is secured from the proposed development to convert an existing on-street parking space to a disabled person parking space should demand arise.

6.10 It is supported that 92 long-stay cycle parking spaces will be provided to the development in excess of London Plan minimum standards. However, the proposed provision of 16 short-stay cycle parking spaces is significantly below the minimum standards set out in Policy T5 of the London Plan where a minimum of 59 short-stay spaces is required and should therefore be provided. All cycle parking should be located in a secure, sheltered and accessible location, and should meet design standards set out in Chapter 8 of the London Cycle Design Standards (LCDS). At least 5% of the cycle parking spaces should be for larger and adapted cycles in line with Chapter 8 of the LCDS.

6.11 After reviewing the revised Deliveries and Servicing Plan (DSP), TfL has the following comments:

- a) TfL understands that servicing will be undertaken from the parking bays in front of the site, in which loading would only be permitted between the hours of 06:30-08:30 and 19:00-21:00. This is supported, subject to approval from Merton Council.
- b) It is understood that most servicing and delivery trips associated with the office will be undertaken via smaller vehicles, which would service the site on-street using existing/modified loading opportunities.
- c) It is supported that refuse collection associated with the office will be undertaken on-street as per the existing situation.
- d) It is understood that the site is expected to receive a net increase of 3 delivery and servicing activities per day, none of which will be in the network peak hours.
- e) It is welcomed that the store manager of the retail unit will liaise with occupiers of the offices to manage the arrival of deliveries and arrange deliveries outside peak periods.

- f) The store manager will encourage tenants to source deliveries from suppliers and operators registered with TfL's Freight Operators Recognition Scheme (FORS). This is also welcomed.
 - g) The DSP should be secured by condition.
- 6.12 It is understood that the Construction Logistics Plan (CLP) and the Travel Plan (TP) remain the same. TfL has no concerns with either the CLP or the TP, subject to approval from Merton Council and bus operations remaining unaffected during construction. The CLP and TP should be secured by condition.
- 6.13 All vehicles should only park/ stop at permitted locations and within the time periods permitted by existing on-street restrictions.
- 6.14 Council's Urban Design Officer (Comments made prior to amendments to the proposal)
- 6.15 Generally I am supportive of this proposal. The architecture is good quality and appropriately contextual, and the height is appropriate and generally in accordance with the Future Wimbledon SPD. It is highly glazed but the brick framing is sufficiently substantial and well detailed to give a good balance to the building. Materials are good quality.
- 6.16 I have two key issues with the proposal. One, is the glazed office entrance which goes the full height of the building, second is the design of the public realm.
- 6.17 The site is quite wide and the street frontage fragmented by service entrances either side. Adding this glazed element fragments the building form and clarity and presence of the building in the street scene. I do not feel it is essential to mark the office entrance in this way, and it adds a verticality that is too thin for the site.
- 6.18 The building is a facade in the street scene and does not need to articulate the corner as if it was on the corner of a street block. It would be appropriate for this element of the building to be incorporated into the brick and glass form of the remainder of the frontage, to make it four bays wide. This would give a better street presence and not emphasise its height too much. The recess on the north side works well. This and the access on the south side serve to provide clear breaks between the adjacent buildings. Currently the main frontage is beginning to get lost among the side gaps and the office glazing, and it shouldn't need to do this.
- 6.19 The building projects further than the existing facade but seems to line up better with the adjacent buildings. The front of the building is let down by a very fragmented approach to the public realm. It is far too fussy and, despite the quality materials, has a dated feel to the design. Separating blocks of pavers with soldier course brick is an example of this. The footway should read as a

single and clear space from building to kerb edge to maximise the sense of width. This will add presence to the building. It is recommended that the whole be re-laid in York stone pavers at a minimum size of 600x450mm in a traditional pattern.

- 6.20 There is also an odd recess/kink in the layout at the entrance to the residential car park. This should be designed out. It is good to see tight radii kerbs for the basement access but the footway needs to be on the level as it crosses it, with small format Yorkstone setts (for strength) to create a seamless and level pedestrian-friendly footway. This should also be proposed for the access to the residential parking (it is in the public highway). Otherwise the setting will be compromised and there will be little quid pro quo for the intensified site use.
- 6.21 Clutter in the footway needs to be kept to a minimum, but half the width is taken up by cycle parking. This needs to be made far more efficient by recessing the spaces into the glazed bays and positioning them at an angle. All other street clutter needs to be identified and rationalised in consultation with the council's highways team.
- 6.22 Council's Transport Planner
- 6.23 Proposed amendments to the delivery and servicing arrangement is acceptable on condition that servicing vehicles are a maximum of 12m in length.
- 6.24 Council's Highway Officer
- 6.25 No objections subject to appropriate conditions including submission of a detailed Construction Logistics Plan.
- 6.26 Council's Climate Change Officer
- 6.27 No objections subject to appropriate conditions and carbon offset financial contribution to be secured by S106 Legal Agreement.
- 6.28 Environmental Health (Noise and Nuisance)
No objections subject to appropriate conditions.
- 6.29 Environmental Health (Air Quality)
No objections subject to appropriate conditions including submission and approval of Construction Management Plan/ Dust Management Plan.
- 6.30 Metropolitan Police (Designing Out Crime Officer)
- 6.31 Have raised some concerns regarding bicycle store, office accommodation accessibility and circulation between the office and retail uses.
- 6.32 Council's Flood Risk Officer
- 6.33 No objections subject to appropriate conditions.

6.34 Thames Water

6.35 Raise no objections regarding impact on waste water, and water networks, sewerage treatment works, and water treatment infrastructure capacity. Have requested a condition requiring the submission of a piling method statement given the works are located within 15m of a strategic sewer.

7. PLANNING CONSIDERATIONS

7.1 Principle of Development

7.2 The Council supports the development of major offices in Wimbledon town centre, which is defined in Policy DM R1 of the Adopted Sites and Policies Plan (July 2014) as offices with more than 1,000sq.m of floorspace. Policy CS.7 of the Core Planning Strategy states that in Wimbledon Town Centre the Council will support high quality offices, especially major development. Policy DM E1 of the Adopted Sites and Policies Plan and Policies Maps (July 2014) states that proposals relating to employment sites will only be supported that (subject to Policy DM E2 and DM E3), retain existing employment land and floor space. The Council will support proposals for the redevelopment of vacant and underused existing employment land and floor space for employment use and proposals for large and major offices in town centres. Policy DM E1 notes that as Wimbledon town centre is tightly bound by residential areas, the possibilities for growth include increasing density on existing sites. This policy states that the council will work with landowners to meet market demand for high quality, well designed large floorplate offices commensurate with Wimbledon's status as a major centre and to take advantage of the internationally recognised Wimbledon 'brand'. The Future Wimbledon SPD (Para. 3.2.1) states that office development providing modern space with large floor plates is limited in the area with demand high and supply limited and as such rents and values are rising. Policy CS 6 of the Core Planning Strategy 2011, seeks to maintain and enhance the retail core of the town centre.

7.3 At a regional level, Policy GG5 of the London Plan (2021) states that to conserve and enhance London's global economic competitiveness and ensure that economic success is shared amongst all Londoners, those involved in planning and development must, among other things, promote the strength and potential of the wider city region, and plan for sufficient employment and industrial space in the right locations to support economic development and regeneration. Policy SD6 also recognises that town centres should be the focus for commercial development beyond the Central Activities Zone (CAZ), and important contributors to the local as well as London-wide economy. Policy E1 seeks to consolidate and, where viable, extend office provision in town centre locations. With regards to retail, Policy E.9 states that a successful, competitive and diverse retail sector, which promotes sustainable access to goods and services for all Londoners, should be supported in line with the wider objectives of this Plan, particularly for town centres.

7.4 At a national level, Paragraph 81 of the NPPF states that 'planning policies and decisions should help create the conditions in which businesses can invest,

expand and adapt. Significant weight should be placed on the need to support economic growth and productivity, taking into account both local business needs and wider opportunities for development'. Therefore significant weight should be placed on the need to support economic growth through the planning system.

7.5 The application site is located in Wimbledon Town centre and has excellent transport links (PTAL rating of 6b), which means it is a highly suitable location for a major commercial (retail/office) development. The proposal would result in a net increase of 1940sqm of GIA overall floorspace, and the applicant has stated in their planning statement that the proposal would generate between 360 – 420 additional jobs, which would help further stimulate the wider Wimbledon economy due to wages being spent locally. It is considered that although there would be a slight decrease in office space to accommodate the retail offering, this is acceptable as the proposed building would re-provide much higher quality office floorspace with well designed large floorplates. The proposal would also extend the retail offer further along this side of Worple Road, which combined with an active frontage would increase the vitality and viability of this part of the town centre.

7.6 Overall, it is considered that the proposal would comply with local, regional and national planning policies, providing a high quality commercial building that is commensurate with Wimbledon's status as a major centre. There is therefore policy support for the proposal in principle.

7.7 Design, Impact on Streetscene and Wider Conservation Area

7.8 Height, Bulk, Massing and Impact on Wider Setting

Policy D9 (Tall Buildings) of the London Plan states that tall buildings are generally those that are substantially taller than their surroundings and cause a significant change to the skyline. Policy D9 states that Boroughs should determine if there are locations where tall buildings may be an appropriate form of development, subject to meeting the other requirements of the Plan, and that any such locations and appropriate tall building heights should be identified on maps in Development Plans.

7.9 Policy D9 of the London Plan states that special attention should be paid to long-range and mid-range views – including the design of the top of the building and the form and proportions of the building. The building should make a positive contribution to the local townscape in terms of legibility, proportions and materiality and the existing and emerging skyline and not adversely affect local or strategic views.

7.10 More specific guidance is outlined in the Tall Buildings Background Paper (2010) which forms part of Merton's Local Development Framework, as an evidence base in support of the Design Policy outlined in the Core Strategy. This states that in Wimbledon Town Centre, tall buildings should contribute to creating a consistent scale of development based on a range of similar but not uniform building heights. These should be determined by reference to surrounding building heights and townscape characteristics.

- 7.11 The Future Wimbledon SPD (November 2020) gives guidance regarding the maximum building heights that would be acceptable on specific sites in Wimbledon Town Centre in terms of both floor count and measured height. The SPD advises that a building of up to eight storeys and a maximum height of 32m can be accommodated on this site. It is considered that given the proposed building would be eight storeys and have a maximum height of approx. 29.8m, that it would comply with the SPD.
- 7.12 The proposed building would be quite prominent from some short and medium range views from within the town centre, and part of the Merton (Wimbledon Hill Road) Conservation Area (mainly from the rear of properties located along Mansel Road). It should be noted that the proposal would not be visible from the main train station square, and important heritage assets such as the Grade II listed Wimbledon Town Hall. In terms of long-range views, it is considered that the proposal would not result in any adverse impact on any protected strategic or local views. The surrounding Wimbledon Town Centre skyline is projected for considerable change, with potential building heights between 8 - 12 storeys identified within the Future Wimbledon SPD on surrounding sites.
- 7.13 The applicant has also provided a number of short and mid-range verified views including looking along Worple Road. It is considered that these views demonstrate that the building is not excessive in terms of its height and size and responds well to the surrounding townscape. The proposed building has a well defined top, middle and base, with the top floor set back reducing its bulk and massing when viewed from the street. The rear of the building has also been reduced with the top two floors reduced in depth, which as a result reduces the mass of the building when viewed from the Conservation Area at the rear.
- 7.14 Design
It is considered that the proposed building would have a well defined top, middle and base, and its design approach is supported by officers. The architecture is high quality and appropriately contextual, whilst the brick framing is sufficiently substantial and well detailed to give a good balance to the building. The proposed materials which comprise buff brickwork, which wraps around the building corner and zinc (with red/copper tone), which is featured on the suspended block over the vehicle access and southwest facing side elevations, are good quality. It is also considered that the recess on the north side of the building works well, as this and the access on the south side serve to provide a clear break between the proposed building and adjacent buildings. The top floor is also set back from the front of the building to further reduce its bulk and massing when viewed from the street.
- 7.15 Heritage
Paragraph 197 of the NPPF states that, in determining proposals affecting heritage assets, account should be taken of: the desirability of sustaining and enhancing their significance; the positive contribution that they can make to sustainable communities and the desirability that new development should make a positive contribution to local character and distinctiveness. London Plan

Policy HC1 requires that development affecting heritage assets should conserve their significance by being sympathetic to their form, scale, materials and architectural detail and that the cumulative impacts from incremental change from development on heritage assets and their setting should be actively managed and that development proposals should avoid harm. Policy DM D4 of the Adopted Sites and Policies Plan and Policies Maps (July 2014) states that all development proposals associated with the borough's heritage assets or their setting will be expected to demonstrate, within a Heritage Statement, how the proposal conserves and where appropriate enhances the significance of the asset in terms of its individual architectural or historic interest and its setting. There are no designated or non-designated heritage assets within the site boundary, therefore no heritage assets will be directly affected through development on the site.

7.16 The Merton (Wimbledon Hill Road) Conservation Area abuts the rear of the application site, with the rear elevations and gardens of properties located on the south side of Mansel Road directly facing the proposal. Although the proposal would be visible from the public realm within the Conservation Area, this would be limited with views restricted to between properties along Mansel Road and from the east end of Worple Road and southern end of Wimbledon Hill Road, which are also in the Conservation Area. It is however considered that the proposal would have an acceptable impact on the Conservation Area, with the top floor set back from the front and rear to reduce its bulk and massing. It is considered that the building is a high quality design and the applicant has submitted a heritage statement and verified views demonstrating how the building will integrate well within the surrounding townscape and conserves the setting of these heritage assets.

7.17 Public Realm

The streetscape at the front of the building is largely formed of an eclectic mix of hard materials and paving types. Red block-work defines the property boundary, whilst beyond this, within the adoptable public pavement space, mixed quality buff paving slabs with block paving details are used. It is considered that the overall paving strategy creates a patchwork of materials and clutters the street. This leads to a poor streetscape experience for pedestrians. A simpler, decluttered pavement would provide a stronger front entrance experience for the new building and aid accessibility for users of the street.

7.18 The applicant has submitted a proposed landscape strategy showing that the Worple Road frontage (both private and public highway) will be paved in Yorkstone. The proposed public realm improvements are supported with the proposed paving modernising, simplifying and decluttering the streetscape. Please note that the proposed public realm improvements are indicative at this stage and subject to approval by the Council's Highways team. The proposed public realm works will however be secured via a S278 Legal Agreement, requiring them to be completed prior to first occupation of the building.

7.19 Residential Amenity

- 7.20 Policy DM D2 of the Adopted Sites and Policies Plan and Policies Maps (July 2014) states that proposals for development will be required to ensure provision of appropriate levels of sunlight and daylight, quality of living conditions, amenity space and privacy, to both proposed and adjoining buildings and gardens. Development should also protect new and existing development from visual intrusion.
- 7.21 Self-contained residential flats are located at Wimbledon Central (Nos. 21 – 33 Worple Road), which bounds the southwest of the site, whilst residential dwellings located on Mansel Road are located to the rear of the site (Nos 10 – 14 Bound the site). Wimbledon Central is a maximum of eight storeys with three six storey wings located at the rear. Commercial units are located at ground floor level with residential located above. The residential properties with windows and balconies/terraces on the northeast facing flank elevation that would be most impacted by the proposal as this element directly faces the proposed building.
- 7.22 The applicant has submitted a daylight and sunlight report which assesses the impact of the proposed development on its surroundings with regards to daylight and sunlight availability to habitable rooms. In this instance the Vertical Sky Component Test (VSC) and Daylight Distribution Test has been applied. The VSC is a measure of the amount of diffuse daylight reaching a window. The BRE advises that where daylight and sunlight is reduced by less than 20% the impact would be negligible, where the reduction is between 20 – 35% the impact is considered to be minor, 35 – 50% the impact is considered to be moderate, and more than 50% the impact is considered to be major. The Daylight Distribution Test is an indication of how good the distribution of daylight is in a room. The BRE guidelines recommends that for an existing room to receive adequate daylight distribution, 80% of the working plane should have a view of the sky.
- 7.22 Wimbledon Central (Nos. 21 – 33 Worple Road)
The VSC test demonstrates that 30 of the 80 windows tested would retain a minimum of 80% of their former value, which means the impact on these windows would be negligible, whilst a further 20 windows would retain between 20 – 35% of their former value, which means the impact would be minor. From the remaining 30 windows, 25 would retain 35 – 50% of their former value, which means there would be a moderate impact on these windows, whilst 5 would lose in excess of 50% of their former value, which means there would be a severe impact on these windows. With regards to the five windows where there would be a severe impact (50+% reduction), it appears that these windows either serve kitchen or balcony windows to the living room. It is considered that the impact on these windows is acceptable in this instance given the rooms which the balconies serve also appear to feature 2 further windows located in the rear elevation, which would not be impacted by the proposal, whilst kitchens given their use are not considered to require the same level of daylight/sunlight as a living room. With regards to the 25 windows where there is a moderate impact (35 – 50% reduction) and 20 windows where there would be a minor impact (20 -35% reduction), these windows also appear to serve similar rooms, with the addition of some bedrooms, which again are

considered not to have the same requirements as a living room given these are generally used at night. It should be noted that 10 windows where there is a moderate reduction in daylight/sunlight are located on a single conservatory at sixth floor level. The impact on this residential unit is considered acceptable given a conservatory is considered to be a secondary living area, with the front (southwest) facing elevation not impacted by the proposal.

- 7.23 A total of 44 rooms were assessed for daylight distribution. This demonstrated that 24 rooms will enjoy levels of daylight distribution that meet the BRE guidance, which includes all of the main living areas within the properties to the rear. Again, similar to the VSC results the rooms that didn't meet BRE guidelines were bedrooms, kitchens and bathrooms, which are not afforded the same level of protection as a living room. Overall, it is considered that given the VSC and daylight distribution results that on balance the impact on daylight/sunlight levels is considered acceptable.
- 7.24 It is considered that the proposal would have an acceptable impact on privacy levels with regards to Wimbledon Central. It should be noted that there are terraces proposed at levels 2, 6 and 7. The terrace at level 6 would be located on the northeast side of the building so will not overlook this building. The terrace at level 2 would be located on the southwest side of the building, whilst the terrace at level 7 is located at the front of the building. A condition will be attached requiring the terrace at level 2 is fully enclosed, and the southwest facing side of the terrace at level 7 is enclosed by a 1.8m high privacy screen to prevent any overlooking of residential units at Wimbledon Central. There is an approx. 20m gap between the rear element of the proposed building, and Wimbledon Central. This element is approximately the same distance to Wimbledon Central when compared to existing so there would be little increase in privacy loss compared to existing up to level 4. Levels 5 – 7 are taller than existing so a condition will be attached requiring windows in the southwest elevation are obscure glazed below 1.8m internal floor height to avoid any additional overlooking.
- 7.25 The proposed building is between approx. 3.7m and 10.1m taller than the existing building, which means it will be more prominent when viewed from Wimbledon Central. It is however considered the building would not be visually intrusive or overbearing given the approx. 20m gap between the rear element of the building above level 2, and the rear wing of Wimbledon Central. The front section of the proposed building would be located approx. 6.2m from the terrace of the level 6 residential unit at Wimbledon Central. It is considered that although this would block views to the northeast from this unit, it would have little impact on views to the southeast, which is considered acceptable as it is considered that it is not realistic or reasonable to expect uninterrupted views in three directions from a roof terrace in a town centre location. The proposed building is also considered to be a much higher quality design than existing, with high quality materials, so the building is an improvement in this respect.
- 7.26 Nos. 2 – 14 Mansel Road
The VSC test demonstrates that all windows tested at Nos. 2 – 14 Mansel Road would retain a minimum of 80% of their former value and as such the impact on

daylight/sunlight levels at these properties would be negligible. The daylight distribution test also demonstrated that all habitable rooms will enjoy daylight distribution that meets the BRE guidance. The proposal would therefore have an acceptable impact on daylight/sunlight levels at these properties.

- 7.27 It is also considered that the proposal would not be visually intrusive or result in an unacceptable level of privacy loss. Since the application was first submitted, the top two floors have been reduced in height, which means the rear of the building is only approx. 3.7m higher than the existing building, whilst the rear elevation has been pulled back a further 2m to an average of approx. 5.8m from the rear boundary compared to the existing approx. 7.7m. It is considered that although there would be some additional impact when viewed from the Mansel Road properties, it would not be excessive, and as such is considered acceptable. A condition will be attached requiring windows in the rear elevation are obscure glazed below 1.8m internal floor height to prevent any overlooking.

7.28 Parking and Traffic

- 7.29 Policy T6 of the London Plan states that Car-free development should be the starting point for all development proposals in places that are (or are planned to be) well connected by public transport. Car-free development has no general parking but disabled persons parking for Blue Badge holders should be provided as set out in Policy T6 .5 on Non-residential disabled persons parking. With regards to Office parking Policy T6.2 states that in well-connected parts of outer London, including town centres, in close proximity to stations and in Opportunity Areas, office developments are encouraged to be car-free. Adequate provision should also be made for efficient deliveries and servicing and emergency access. With regards to cycle parking, Policy T5 states that this should be designed and laid out in accordance with the guidance contained in the London Cycling Design Standards, and that development proposals should demonstrate how cycle parking facilities will cater for larger cycles, including adapted cycles for disabled people.
- 7.30 At a local level Policy CS.18 promotes active transport and encourages design that provides attractive, safe, covered cycle storage, cycle parking and other facilities (such as showers, bike cages and lockers). Policy CS.20 of the Core Planning Strategy states that the Council will require developers to demonstrate that their development will not adversely affect pedestrian and cycle movements, safety, the convenience of local residents or the quality of bus movement and/or facilities; on-street parking and traffic management. This is endorsed in Policies DM T1 and DM T3 of the 2014 Sites and Policies Plan. Developments should also incorporate adequate facilities for servicing to ensure loading and unloading activities do not have an adverse impact on the public highway.
- 7.31 The application site is well connected and has excellent public transport links (PTAL rating of 6b). The site is served by rail services from Wimbledon station and a number of bus routes run along Worple Road. The proposal does not include any car parking for employees (net loss of 68 spaces), and this is

considered acceptable given the sites highly accessible location. Given the site is within a Controlled Parking Zone (W1) and has a PTAL rating of 6b, the proposal is to be 'permit free' preventing future employees from obtaining parking permits. Policy CS.20 of the Core Planning Strategy states that the Council will support permit free developments in areas within CPZ's benefiting from good access to public transport (PTAL 4-6).

- 7.32 Policy T6.5 states that standards for non-residential disabled persons parking are based on a percentage of the total number of parking bays, however, all non-residential elements should still provide access to at least one on or off-street disabled persons parking bay. The development provides no Blue Badge (BB) parking however this is considered acceptable in this instance given the constraints of the site. It is however recommended that a financial contribution is secured to convert an existing on-street parking space to a disabled person parking space should demand arise in future.
- 7.33 The application as originally submitted proposed to service the development off-street. However, this was considered unacceptable by officers due to the difficulty and disruption of manoeuvring heavy goods vehicles into the site from Worple Road. After discussions with officers it is now proposed to service the site on-street. This would involve shared use of the three parking bays directly in front of the site (on the west side of the existing site access) with loading only being permitted between the hours of 7:00am-8:30am and 7:00pm-9:00pm (Mondays to Saturdays). Between the hours 8:30am-7:00pm (Mondays to Saturdays) and 10:00am-4:00pm (Sundays) the bays would be available for Pay and Display parking only (i.e. no loading); whilst overnight no restrictions would apply. The applicant has submitted a Transport Assessment that states the office use is likely to generate a similar number of servicing trips as existing, whilst the retail element would likely generate 3 daily trips, which means overall there would only be a small increase in servicing trips. The applicant has submitted an updated Servicing and Delivery Plan which includes swept path analysis that demonstrates that a 12m vehicle can safely enter and exit the proposed on-street parking area in forward gear. Servicing and delivery vehicles will be restricted to 12m in size and this will be secured by S106 Agreement.
- 7.34 The proposed building would provide 7330sqm of GIA floorspace comprising 2108sqm of retail (Use Class E(a)) at ground and first floor level, and 5069sqm of office (Use Class E(g)(i)) above. An additional 153sqm, which comprises some plant and basement access areas, would be shared space. This means the proposal should provide a total of 78 long stay cycle spaces and 57 short stay cycle spaces to comply with London Plan Policy T.5. The proposed development would comply with this policy, providing a total of 92 long stay cycle spaces at basement level, which are accessed using a power assisted wheel track. Although the proposed number of short stay cycle spaces (16) does not comply with policy, it is considered that the proposed provision is acceptable in this instance given the lack of available space outside the front of the building. However, given there is a shortfall, the applicant will be required to provide a financial contribution of £12,300 (41 x £300 per short stay cycle space) for short stay cycle provision in the local area. The cycle storage is also

secure, covered and other facilities such as showers and lockers are provided. A condition, will be attached requiring full details of the cycle parking proposed, which shall include a requirement that at least 5% of the cycle parking spaces are for larger and adapted cycles.

7.35 The Council's Transport and Highways officers have assessed the application and consider it acceptable subject to appropriate conditions.

7.36 **Sustainability and Energy**

7.37 The application includes an Energy Sustainability Statement indicating that the development is targeting a BREEAM rating of Excellent for the Offices and Very Good for the retail element which meets Merton's minimum requirements. The GLA's Guidance on preparing energy assessments and the new London Plan require all major non-domestic schemes to achieve at least a 15% improvement against Building Regulations through energy efficiency alone. Based on the energy statement provided, the proposed development achieves this target with a 20.74% improvement through energy efficiency alone. District heating networks have been discounted due to the lack of an existing or planned district heating network in the vicinity of the site. The applicant has confirmed that provisions for connection to future District Heating Schemes will be provided. This will be secured via condition.

7.38 A carbon offset financial contribution of £104,439, which unless agreed in writing should be paid upon commencement of the development. The Council's Climate Change Officer has assessed the application and has confirmed that the application would comply with policies on climate change and water usage. Appropriate conditions are recommended.

7.39 **Basement Construction and Flood Risk**

7.40 The existing building comprises a basement area, which extends up to the highway edge. The proposed building would feature a larger basement area, which extends to the rear of the site. With regards to flood risk, the applicant has provided a drainage report. The report includes an in-depth assessment of the risk of flooding and provides adequate mitigation measures including attenuation tanks, blue roof, and permeable paving, which will help prevent run-off rates exceeding capacity. The site lies within Flood Zone 1 wherein principle the proposed development (including basement) is acceptable. Thames Water and the Council's Flood Engineer have assess the proposal and raise no objections subject to conditions.

7.41 The submitted Basement Impact Assessment documents also demonstrate that the proposed basement works can be undertaken safely without adversely affecting the surrounding built and natural environment. A condition will be attached requiring the submission of detailed demolition, design and construction method statements.

7.42 **Trees**

Policy DM O2 of the Adopted Sites and Policies Plan and Policies Maps (July 2014) states that development will only be permitted if it will not damage or destroy any tree which has significant amenity value, is in a conservation area or is subject to a Tree Preservation Order (TPO). Policy G 7 of the London Plan states that development proposals should ensure that, wherever possible, existing trees of value are retained.

- 7.43 There are three trees located immediately to the rear of the site in the rear gardens of Mansel Road. One tree has been given a 'B' category rating whilst the other two have been given a 'C' category rating. The applicant has submitted an Arboricultural Implications Assessment and Tree Protection Plan, which demonstrates that the proposal would not have a detrimental impact on these trees on condition that the recommended tree protection measures are put in place during construction works.

7.44 Air Quality

- 7.45 The applicant has submitted an Air Quality Assessment (AQA) report which considers the potential impacts on local air quality associated with the construction and operation of the proposed development.

- 7.46 The proposed development has the potential to expose future users to elevated pollution levels and impact existing air quality in the vicinity of the site during operation. Dispersion modelling was therefore undertaken using ADMS-Roads in order to predict pollutant concentrations as a result of emissions from the local highway network. Results were subsequently verified using local monitoring data.

- 7.47 Impacts on NO₂ and PM₁₀ concentrations as a result of operational phase road vehicle exhaust emissions were predicted to be negligible at all sensitive receptor locations. The results of the dispersion modelling assessment indicated NO₂ concentrations were below the relevant 1-hour mean AQO at the proposed retail space.

- 7.48 The Proposed Development is considered to be air quality neutral in regard to both building emissions and transport emissions in line with the LPG Air Quality Neutral Consultation Draft. The assessment has defined appropriate mitigation measures to reduce the level of dust, which may temporarily occur during demolition and construction activities. The Council's Environmental Health Officer has assessed the application and raises no objections subject to conditions including submission of Construction Management Plan/ Dust Management Plan.

7.49 Fire Safety

- 7.50 The applicant has submitted a fire statement which includes the following:
- Assessment of the building's means of escape requirements, demonstrating adequate arrangements are in place to provide warning and evacuate the building safely.

- The internal fire spread requirements of the building, including structural fire protection, compartmentation and surface spread of flame;
- Assessment of external fire spread;
- Provision of adequate access and facilities for the fire service; and,
- That appropriate operational fire safety management arrangements will be in place.

8. ENVIRONMENTAL IMPACT ASSESSMENT

- 8.1 The application does not constitute Schedule 1 or Schedule 2 development. Accordingly, there are no requirements in terms of EIA submission.

9. LOCAL FINANCIAL CONSIDERATIONS

- 9.1 The proposal would result in a net gain in gross floor space and as such will be liable to pay a Community Infrastructure Levy (CIL). The funds will be spent on the Crossrail project, with the remainder spent on strategic infrastructure and neighbourhood projects.

10. CONCLUSION

- 10.1 No.19 Worple Road is located in Wimbledon Town centre and has excellent transport links (PTAL rating of 6b), which means it is a highly suitable location for a major commercial development. The proposal would provide an enlarged, modernised and highly sustainable commercial (office/retail) building with well designed large floorplates commensurate with Wimbledon's status as a major centre. It is considered that the proposal would respect its context in terms of its scale and massing, would be of a high quality design which contributes to local distinctiveness. New major office floorspace proposals are encouraged within Wimbledon Town Centre and the proposal would be compliant with policy. The high quality design is such that officers are satisfied that it would not be a visually harmful building and would be commensurate with the desires for intensification of development in the town centre as set out in the Future Wimbledon SPD. The impact on residential amenity, transport and highways, sustainability, and flood risk, is considered to be acceptable subject to the imposition of suitable conditions. It is therefore recommended that planning permission be granted subject to the conditions and heads of terms set out below.

RECOMMENDATION

GRANT PLANNING PERMISSION subject to the completion of a S106 agreement covering the following heads of terms:

- 1) S278 agreement to be entered into for public realm improvements
- 2) Carbon Offset Financial Contribution (£104,439)
- 3) Financial contribution for cycle parking in the local vicinity (£12,300)
- 4) Financial contribution for conversion of on-street parking bay to disabled bay (To be confirmed)
- 5) Permit free
- 6) Restriction of delivery and service vehicle size
- 7) Paying the Council's costs for monitoring the travel plan over five years (£2000)
- 8) Paying the Council's legal and professional costs in drafting, completing and monitoring the legal agreement.

And subject to the following conditions:

1. A.1 (Commencement of Development)
2. A.7 (Approved plans)
3. B.1 (External Materials to be Approved)
4. B.4 (Details of Surface Treatment)
5. B.5 (Details of wall/fences)
6. No external windows and doors shall be installed until detailed drawings at 1:20 scale of all external windows and doors, including materials, set back within the opening, finishes and method of opening have been submitted to and approved by the local planning authority. Only the approved details shall be used in the development hereby permitted.

Reason: To ensure a satisfactory standard of development in accordance with the following Development Plan policies for Merton: policies 7.5 and 7.6 of the London Plan 2016, policy CS14 of Merton's Core Planning Strategy 2011 and policies DM D1 and D2 of Merton's Sites and Policies Plan 2014.

7. Before the development hereby permitted is first occupied, the windows in the southwest and rear facing elevations at level 5 and above shall be glazed with obscured glass and shall be maintained as such thereafter.

Reason: To safeguard the amenities and privacy of the occupiers of adjoining properties and to comply with the following Development Plan policies for Merton: policies D3 and D4 of the London Plan 2021, policy CS14 of Merton's Core Planning Strategy 2011 and policies DM D2 and D3 of Merton's Sites and Policies Plan 2014.

8. The offices shall not be occupied until a scheme of details of screening of the balconies at levels 2 and 7 have been submitted for approval to the Local Planning Authority. The development shall not be occupied until the scheme has been approved and implemented in its approved form and those details shall thereafter be retained for use at all times from the date of first occupation.

Reason: To safeguard the amenities and privacy of the occupiers of adjoining properties and to comply with the following Development Plan policies for Merton: policies D3 and D4 of the London Plan 2021, policy CS14 of Merton's Core Planning Strategy 2011 and policies DM D2 and D3 of Merton's Sites and Policies Plan 2014.

9. C.6 (Refuse and Recycling - Details to be submitted)
10. D.11 (Construction Times)
11. H.6 (Cycle Parking – Details to be Submitted)
12. H.8 (Travel Plan)
13. Development shall not commence until a working method statement relating to the demolition and construction phase has been submitted to and approved in writing by the Local Planning Authority to accommodate:
 - (i) Parking of vehicles of site workers and visitors;
 - (ii) Loading and unloading of plant and materials;
 - (iii) Storage of construction plant and materials;
 - (iv) Wheel cleaning facilities
 - (v) Control of dust, smell and other effluvia;
 - (vi) Control of surface water run-off.

No development shall be carried out except in full accordance with the approved method statement.

Reason: To ensure the safety of pedestrians and vehicles and the amenities of the surrounding area and to comply with the following Development Plan policies for Merton: policies T4 and T7 of the London Plan 2021, policy CS20 of Merton's Core Planning Strategy 2011 and policy DM T2 of Merton's Sites and Policies Plan 2014.

14. The development shall comply with the approved Delivery and Servicing Plan (Ref: liwimb/1909057) dated 10th June 2022 (as amended by email received 26th July 2022). The approved measures shall be maintained, in accordance with the Plan, for the duration of the use, unless the prior written approval of the Local Planning Authority is obtained to any variation.

Reason: To ensure the safety of pedestrians and vehicles and the amenities of the surrounding area and to comply with the following Development Plan policies for Merton: policies T4 and T7 of the London Plan 2021, policy CS20 of Merton's Core Planning Strategy 2011 and policies DM T2, T3 and T5 of Merton's Sites and Policies Plan 2014.

15. Prior to the commencement of the development hereby permitted, a Construction Logistics Plan shall be submitted to and approved in writing by the Local Planning Authority. The approved measures shall be implemented prior to the first occupation of the development hereby permitted and shall be so maintained for the duration of the use, unless the prior written approval of the Local Planning Authority is first obtained to any variation.

Reason: To ensure the safety of pedestrians and vehicles and the amenities of the surrounding area and to comply with the following Development Plan policies for Merton: policies T4 and T7 of the London Plan 2021, policy CS20 of Merton's Core Planning Strategy 2011 and policy DM T2 of Merton's Sites and Policies Plan 2014.

16. Prior to the commencement of development, including demolition, a detailed Demolition and Construction Environmental Management Plan (DCEMP) shall be submitted to and approved in writing by the Local Planning Authority. The DCEMP shall include:

- a) An Air quality management plan that identifies the steps and procedures that will be implemented to minimise the creation and impact of dust and other air emissions resulting from the site preparation, demolition, and groundwork and construction phases of the development. To include continuous dust monitoring.

- b) Construction environmental management plan that identifies the steps and procedures that will be implemented to minimise the creation and impact of noise, vibration, dust and other air emissions resulting from the site preparation, demolition, and groundwork and construction phases of the development.

- c) The development shall not be implemented other than in accordance with the approved scheme, unless previously agreed in writing by the Local Planning Authority.

Reason: To ensure the development does not raise local environment impacts and pollution.

17. All Non-Road Mobile Machinery (NRMM) of net power of 37kW and up to and including 560kW used during the course of the demolition, site preparation and construction phases shall comply with the emission standards set out in chapter 7 of the GLA's supplementary planning guidance "Control of Dust and Emissions During Construction and Demolition" dated July 2014 (SPG), or subsequent guidance. Unless it complies with the standards set out in the SPG, no NRMM shall be on site, at any time, whether in use or not, without the prior written consent of the local planning authority. The developer shall keep an up to date list of all NRMM used during the demolition, site preparation and construction phases of the development on the online register at <https://nrmm.london/>

Reason: To manage and prevent further deterioration of existing low quality air across London in accordance with London Plan policies GG3 and SI1, and NPPF 181.

18. F.1 (Landscaping/Planting Scheme)

19. The building shall not be occupied until details of public realm improvement works have been submitted and approved by the Local Planning Authority. The works shall be carried out prior to first occupation of the approved building and shall be permanently retained thereafter.

Reason: To achieve a high quality public realm in accordance with the following Development Plan policies for Merton: policy CS14 of Merton's Core Planning Strategy 2011 and policies DM D1 and D2 of Merton's Sites and Policies Plan 2014.

20. The development hereby permitted shall incorporate security measures to minimise the risk of crime and to meet the specific security needs of the development in accordance with Secured by Design. Details of these measures shall be submitted to and approved in writing by the local planning authority prior to first occupation of the development and shall be implemented in accordance with the approved details prior to occupation.

Reason: In order to achieve the principles and objectives of Secured by Design to improve community safety and crime prevention in accordance with Policy: Chapters 01B & 01C Merton New Local Plan, Policy D11 London Plan, Section 17 Crime and Disorder Act 1988 and National Planning Policy Framework (NPPF).

21. Prior to occupation a Secured by Design final certificate or its equivalent from the South West Designing Out Crime office shall be submitted to and approved by the Local Planning Authority.

Reason: In order to achieve the principles and objectives of Secured by Design to provide a safer environment for future residents and visitors to the site and reduce the fear of crime in accordance with Policy: Chapters 01B & 01C Merton New Local Plan, Policy D11 London Plan, Section 17 Crime and Disorder Act 1988 and National Planning Policy Framework (NPPF).

22. No development shall occur until a site investigation is undertaken to consider the potential for contaminated-land, and then if necessary, a detailed remediation scheme described to make the site suitable for, intended use by removing unacceptable risks to sensitive receptors, is submitted to and approved by the Local Planning Authority prior to commencement of works.

Reason: To protect the health of future users of the site in accordance with policy DM EP4 of Merton's sites and policies plan 2014.

23. Prior to first occupation, the remediation shall be completed and a verification report, produced on completion of the remediation, shall be submitted to the approval of the LPA.

Reason: To protect the health of future users of the site in accordance with policy DM EP4 of Merton's sites and policies plan 2014.

24. The recommendations as specified in the Acoustics Consultants Ltd, Noise Assessment Report, Issue No.9, dated 19th July 2022 shall be implemented as a minimum standard. A post construction noise survey shall be conducted and remedial measures implemented should be submitted criteria fail to be achieved, first being agreed by the LPA.

Reason: To protect the health of future users of the site in accordance with policy DM EP2 of Merton's sites and policies plan 2014.

25. Loading and unloading is only permitted between the hours of 07:00-08:30 and 19:00-21:00 Mondays to Saturdays, and not on Sundays or Bank holidays.

Reason: To safeguard the amenities of the area and the occupiers of neighbouring properties and ensure compliance with the following Development Plan policies for Merton: policies D14 and T7 of the London Plan 2021 and policy DM EP2 of Merton's Sites and Polices Plan 2014.

26. Unless otherwise agreed in writing by the Local Planning Authority, no part of the development hereby approved shall be used or occupied until evidence demonstrating that the development has achieved CO2 reductions in accordance with those outlined in the energy statement (dated 19th July 2022), has been submitted to and acknowledged in writing by the Local Planning Authority.

Reason: To ensure that the development achieves a high standard of sustainability and makes efficient use of resources and to comply the following Development Plan policies for Merton: policy SI2 of the London Plan 2021 and policy CS15 of Merton's Core Planning Strategy 2011.

27. No development shall commence until the applicant submits to, and has secured written approval from, the Local Planning Authority evidence demonstrating that the development has been designed to enable connection of the site to an existing or future district heating network, in accordance with the Technical Standards of the London Heat Network Manual (2014).

Reason: To demonstrate that the site heat network has been designed to link all building uses on site (domestic and non-domestic), and to demonstrate that sufficient space has been allocated in the plant room for future connection to wider district heating, in accordance with London Plan policies SI2 and SI3.

28. In order to demonstrate compliance with the 'be seen' post-construction monitoring requirement of Policy SI 2 of the London Plan, the legal Owner shall at all times and all in all respects comply with the energy monitoring requirements set out in points a, b and c below. In the case of non-compliance the legal Owner shall upon written notice from the Local Planning Authority immediately take all steps reasonably required to remedy non-compliance.

- a) Prior to commencement of development hereby permitted, the Owner is required to submit to the GLA accurate and verified estimates of the 'be seen' energy performance indicators, as outlined in Chapter 3 'Planning stage' of the GLA 'Be seen' energy monitoring guidance document, for the consented development. This should be submitted to the GLA's monitoring portal in accordance with the 'Be seen' energy monitoring guidance.
- b) Once the as-built design has been completed (upon commencement of RIBA Stage 6) and prior to the building(s) being occupied (or handed over to a new legal owner, if applicable), the legal Owner is required to provide updated accurate and verified estimates of the 'be seen' energy performance indicators for each reportable unit of the development, as per the methodology outlined in Chapter 4 'As-built stage' of the GLA 'Be seen' energy monitoring guidance. All data and supporting evidence should be uploaded to the GLA's monitoring portal. The owner should also confirm that suitable monitoring devices have been installed and maintained for the monitoring of the in-use energy performance indicators, as outlined in Chapter 5 'In-use stage' of the GLA 'Be seen' energy monitoring guidance document.
- c) Upon completion of the first year of occupation following the end of the defects liability period (DLP) and for the following four years, the legal Owner is required to provide accurate and verified annual in-use energy performance data for all relevant indicators under each reportable unit of the development as per the methodology outlined in Chapter 5 'In-use stage' of the GLA 'Be seen' energy monitoring guidance document. All data and supporting evidence should be uploaded to the GLA's monitoring portal. This condition will be satisfied after the legal Owner has reported on all relevant indicators included in Chapter 5 'In-use stage' of the GLA 'Be Seen' energy monitoring guidance document for at least five years.

Reason: In order to ensure that actual operational energy performance is minimised and demonstrate compliance with the 'be seen' post-construction monitoring requirement of Policy SI 2 of the London Plan.

29. Unless otherwise agreed in writing by the Local Planning Authority, a Post-Construction Review Certificate issued by the Building Research Establishment or other equivalent assessors confirming that the offices and the retail unit have achieved a BREEAM rating of not less than the standards equivalent to 'Excellent' and 'Very Good' respectively shall be submitted to and acknowledged in writing by the Local Planning Authority within 6 months of first occupation.

Reason: To ensure that the development achieves a high standard of sustainability and makes efficient use of resources and to comply the following Development Plan policies for Merton: policy CS15 of Merton's Core Planning Strategy 2011.

30. Notwithstanding the provisions of the Town and Country Planning (General Permitted Development) (England) Order 2015 (or any Order revoking and re-enacting that Order with or without modification), floor level 2 and above of the development shall be used for office use (Use Class E(g)(i)) and for no other purpose, without planning permission first obtained from the Local Planning Authority.

Reason: To ensure that there is an adequate supply of suitable sites and premises in locations that optimise opportunities and co-locational advantages for offices and minimise negative effects on other users and to comply with the following Development Plan policies for Merton: Policy DM E2 of Merton's Sites and Policies Plan 2014 and policy CS12 of the Core Planning Strategy 2011.

31. No development shall commence prior to submission and approval by the Local Planning Authority of a Basement Construction Statement comprising the following:

- a) Ground Movement Analysis (Vertical and Horizontal) including any heave or settlement analysis, and Damage Category Assessment with detailed calculations.
- b) Site Specific Ground Investigation Report with borehole logs and an interpretative report with recommendations for the foundation type and design
- c) Approval in Principle in accordance with CG 300 (DMRB) signed off by LBM Highways.
- d) Detailed Demolition Method Statement submitted by the Contractor responsible for the demolition of the existing property.
- e) Detailed Construction Method Statement produced by the respective Contractors responsible for the sheet piling, excavation and construction of the permanent retaining wall. This shall be reviewed and agreed by the Structural Engineer designing the basement.
- f) Design calculations of the temporary works supporting the highway and adjoining properties to facilitate excavation.
- g) Detail design calculations of the permanent retaining wall retaining the highway has to be submitted. The calculations shall be carried out in accordance with Eurocodes. We recommend assuming full hydrostatic pressure to ground level and using a highway surcharge of 20 KN/m² for the design of the retaining wall supporting the highway.
- h) Temporary retaining wall drawings such as pile plan, section and construction sequence.
- i) Permanent retaining wall drawings including plan, section and construction sequence.

j) Movement monitoring report produced by specialist surveyors appointed to install monitoring gauges to detect any movement of the highway/neighbouring properties from start to completion of the project works. The report should include the proposed locations of the horizontal and vertical movement monitoring, frequency of monitoring, trigger levels, and the actions required for different trigger alarms.

32. No piling shall take place until a Piling Method Statement (detailing the depth and type of piling to be undertaken and the methodology by which such piling will be carried out, including measures to prevent and minimise the potential for damage to subsurface sewerage infrastructure, and the programme for the works) has been submitted to and approved in writing by the local planning authority in consultation with Thames Water. Any piling must be undertaken in accordance with the terms of the approved piling method statement

Reason: The proposed works will be in close proximity to underground sewerage utility infrastructure. Piling has the potential to significantly impact / cause failure of local underground sewerage utility infrastructure.

33. Prior to the commencement of the construction phase (excluding demolition), detailed plans and sections demonstrating that the threshold levels for all access points (including basements) are set at a minimum of 300mm above the existing road levels, shall be submitted to and approved in writing by the local planning authority.

Reason: To reduce the risk of surface and foul water flooding to the proposed development and future users, and ensure surface water and foul flood risk does not increase offsite in accordance with Merton's policies CS16, DMF2 and the London Plan policy SI 13.

34. Prior to the commencement of the construction phase (excluding demolition), a detailed scheme for the provision of surface and foul water drainage shall be submitted to and approved in writing by the local planning authority for the development. The drainage scheme will dispose of surface water by means of a sustainable drainage system (SuDS) based on the 100yr plus 40% climate change event, at the agreed runoff rate (no more than 2l/s) in accordance with drainage hierarchy contained within the London Plan Policy (SI 13 and SPG) and the advice contained within the National SuDS Standards. A maintenance plan should also be included to ensure that they last the lifetime of the development.

Reason: To reduce the risk of surface and foul water flooding to the proposed development and future users, and ensure surface water and foul flood risk does not increase offsite in accordance with Merton's policies CS16, DMF2 and the London Plan policy SI 13

35. Condition: Prior to the commencement of the construction phase (excluding demolition), the applicant shall submit a detailed proposal on how drainage and groundwater will be managed and mitigated during and post construction (permanent phase), for example through the implementation of passive

drainage measures around the basement structure, waterproofing and drainage.

Reason: To reduce the risk of surface and foul water flooding to the proposed development and future users and ensure surface water and foul flood risk does not increase offsite in accordance with Merton's policies CS16, DMF2 and the London Plan policy SI 13.

36. Condition: Prior to the commencement of the construction phase (excluding demolition), the detailed design and specification for the blue roofs shall be submitted to and approved in writing by the Local Planning Authority. The design shall be carried out as approved, retained and maintained by the applicant in perpetuity thereafter.

Reason: To reduce the risk of surface and foul water flooding to the proposed development and future users and ensure surface water and foul flood risk does not increase offsite in accordance with Merton's policies CS16, DMF2 and the London Plan policy SI 13.

37. The retail use (Use Class E(a)) hereby permitted shall operate only between the hours of 0700 to 2300 Monday to Saturday, and 1000 to 1700 on Sunday and Bank Holidays.

Reason: To safeguard the amenities of surrounding area and to ensure compliance with the following Development Plan policies for Merton: policies D4 and D14 of the London Plan 2021, policy CS7 of Merton's Core Planning Strategy 2011 and policy DM EP2 of Merton's Sites and Policies Plan 2014.

38. No development [including demolition] pursuant to this consent shall commence until the tree protection measures set out in TREE SURVEY, ARBORICULTURAL IMPACT ASSESSMENT & TREE PROTECTION PLAN Rev:3 dated 24th August 2021 have been implemented on site and in accordance with the recommendations and guidance set out in BS 5837:2012. The details and measures as approved shall be retained and maintained, until the completion of all site operations.

Reason: To protect and safeguard the existing retained trees in accordance with the following Development Plan policies for Merton: policy G7 of the London Plan 2021, policy CS13 of Merton's Core Planning Strategy 2011 and policy O2 of Merton's Sites and Policies Plan 2014.

39. Site supervision: The requirements of condition 38 shall include the retention of an arboricultural expert to supervise, monitor and report to the LPA not less than monthly the status of all tree works and tree protection measures throughout the course of the demolition and construction period. At the conclusion of the construction period the arboricultural expert shall submit to the LPA a satisfactory completion statement to demonstrate compliance with the approved protection measures.

Reason: To protect and safeguard the existing retained trees in accordance with the following Development Plan policies for Merton: policy G7 of the London Plan 2021, policy CS13 of Merton's Core Planning Strategy 2011 and policy DMO2 of Merton's Sites and Policies Plan 2014.

40. Details of the proposed external plant, ventilation and extraction equipment shall be submitted to and approved in writing by the Local Planning Authority prior to occupation of the building hereby permitted. The development shall be carried out in accordance with the approved details”.

Reason: To safeguard the amenities of surrounding area and to ensure compliance with the following Development Plan policies for Merton: policies D4 and D14 of the London Plan 2021, policy CS7 of Merton's Core Planning Strategy 2011 and policy DM EP2 of Merton's Sites and Policies Plan 2014.

41. INFORMATIVE: Carbon emissions evidence requirements for Post Construction stage assessments must provide:
- Detailed documentary evidence confirming the Target Emission Rate (TER), Building Emission Rate (BER) and percentage improvement of BER over TER based on 'As Built' BRUKL outputs and bespoke model outputs; AND
 - A copy of the Building Regulations Output Document from the approved software and the bespoke modelling outputs based on the agreed bespoke modelling methodology. The output documents must be based on the 'as built' stage of analysis and must account for any changes to the specification during construction.
- AND, where the developer has used SAP 10 conversion factors:
- The completed Carbon Emissions Reporting Spreadsheet based on the 'As Built' BRUKL outputs.
- AND, where applicable:
- MCS certificates and photos of all installed renewable technologies.
42. INF9 (Works on the Public Highway)
43. INF12 (Works affecting the public highway)
44. INFORMATIVE: Thames Water will aim to provide customers with a minimum pressure of 10m head (approx. 1 bar) and a flow rate of 9m/minute at the point where it leaves Thames water pipes. The developer should take account of this minimum pressure in the design of the proposed development.
45. INFORMATIVE: No surface water runoff should discharge onto the public highway including the public footway or highway. When it is proposed to connect to a public sewer, the site drainage should be separate and combined at the final manhole nearest the boundary. Where the developer proposes to discharge to a public sewer, prior approval from Thames Water Developer

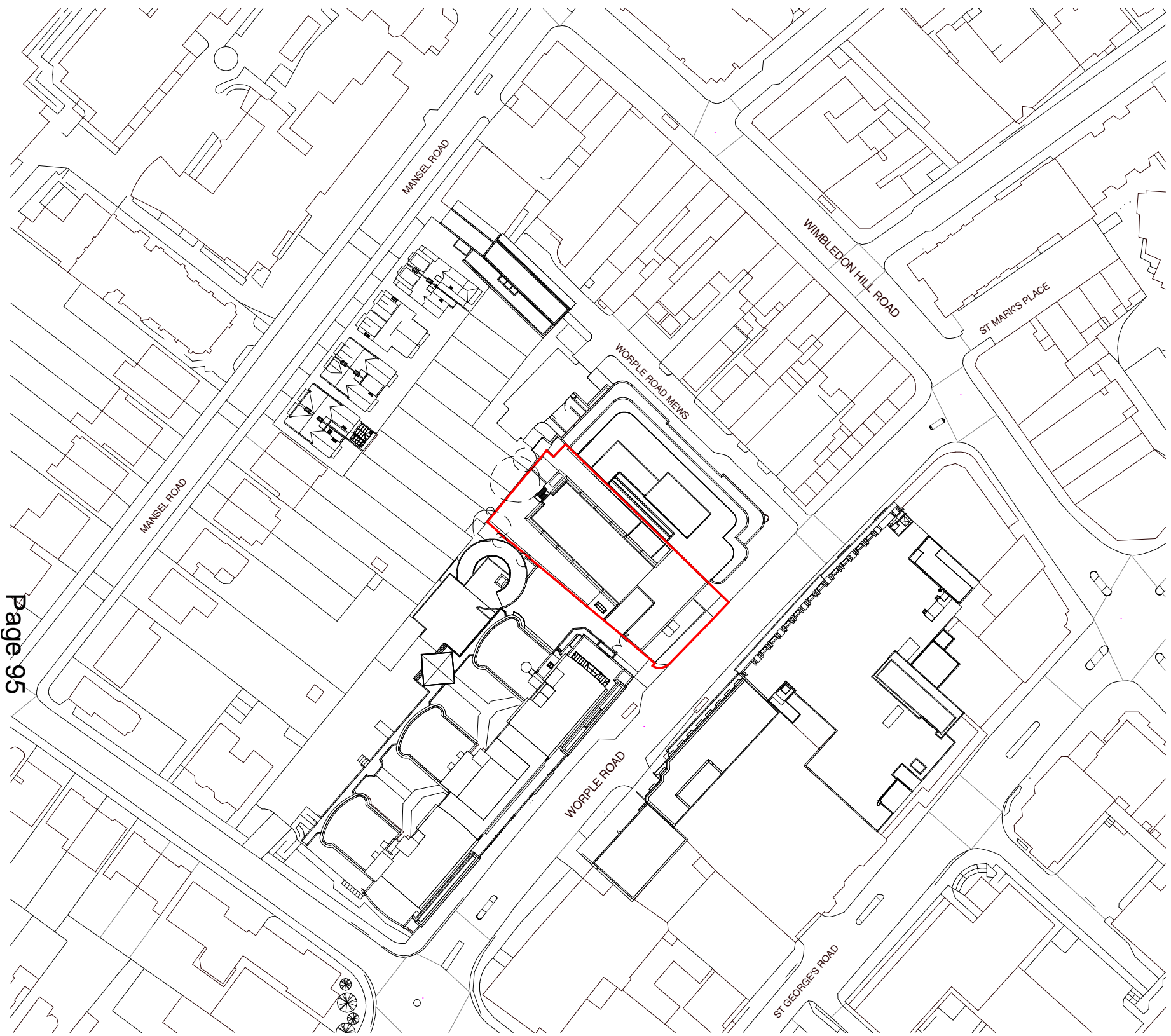
Services will be required (contact no. 0845 850 2777). No waste material, including concrete, mortar, grout, plaster, fats, oils and chemicals shall be washed down on the highway or disposed of into the highway drainage system.

NORTHGATE SE GIS Print Template



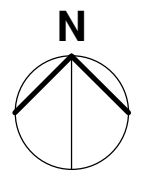
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| Revision | Description | Author | Checker | Date |
|----------|---------------------|--------|---------|----------|
| P1 | Planning Submission | LC | PB | 17.11.20 |



 Application Boundary

Page 95



Site Location Plan

1 : 1250

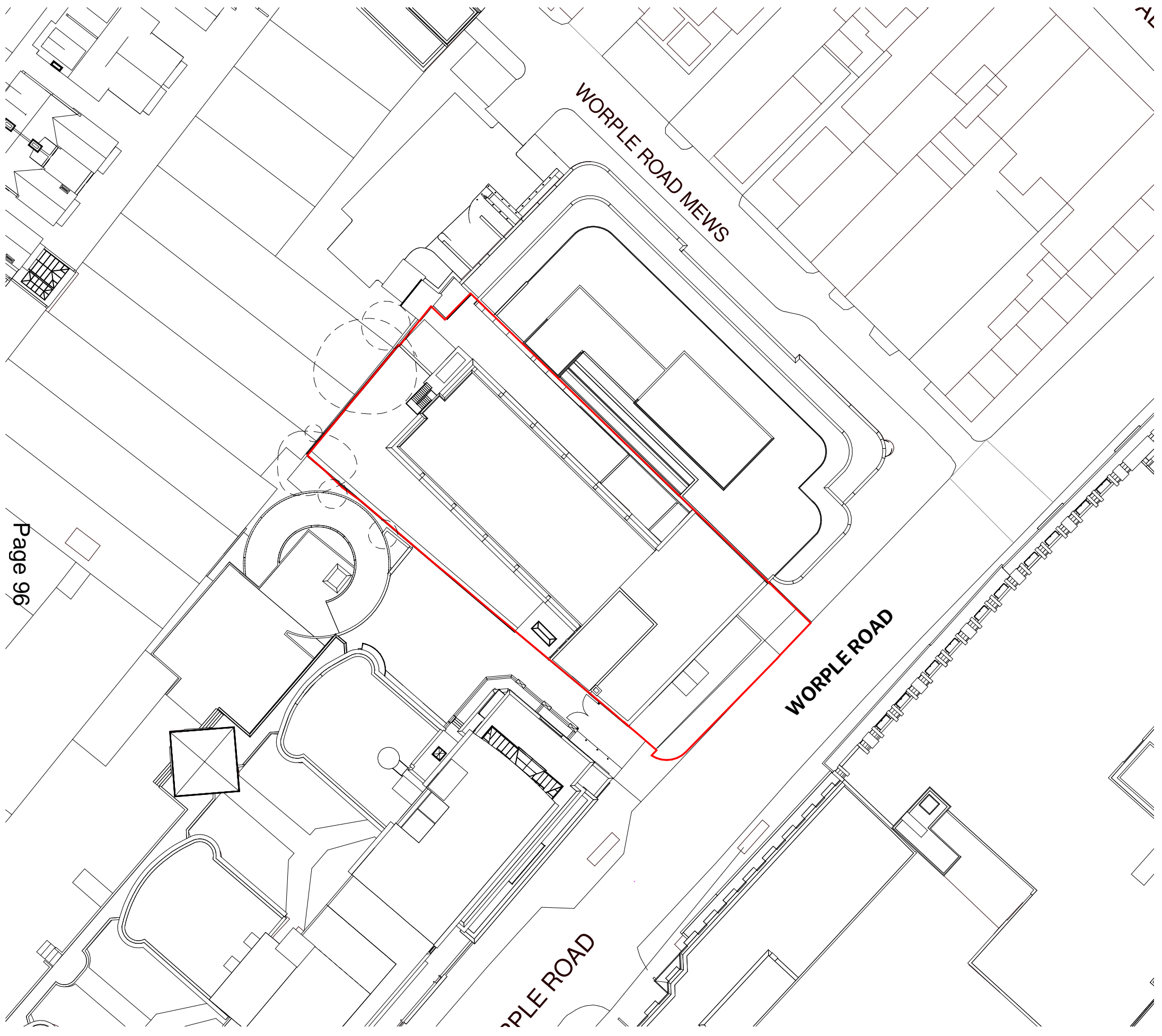
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| | | | | |
|---|---------------------------|--------|-----------|---------------------|
| Project | | | | |
| 07758 - 19 Worple Road | | | | |
| Drawing Title | | | | |
| Site Location Plan | | | | |
| Project No. | Drawing No. | Status | Revision | Scale at A3 |
| 07758 | ZZ-XX-DR-A-91-1001 | | P1 | As indicated |
| File Reference: 07758-SPACE-ZZ-XX-DR-A-91-1001 | | | | |

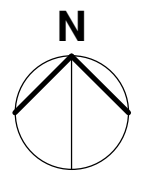


| Revision | Description | Author | Checker | Date |
|----------|---------------------|--------|---------|----------|
| P1 | Planning Submission | LC | PB | 17.11.20 |



 Application Boundary

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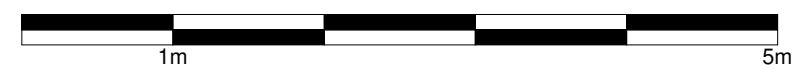


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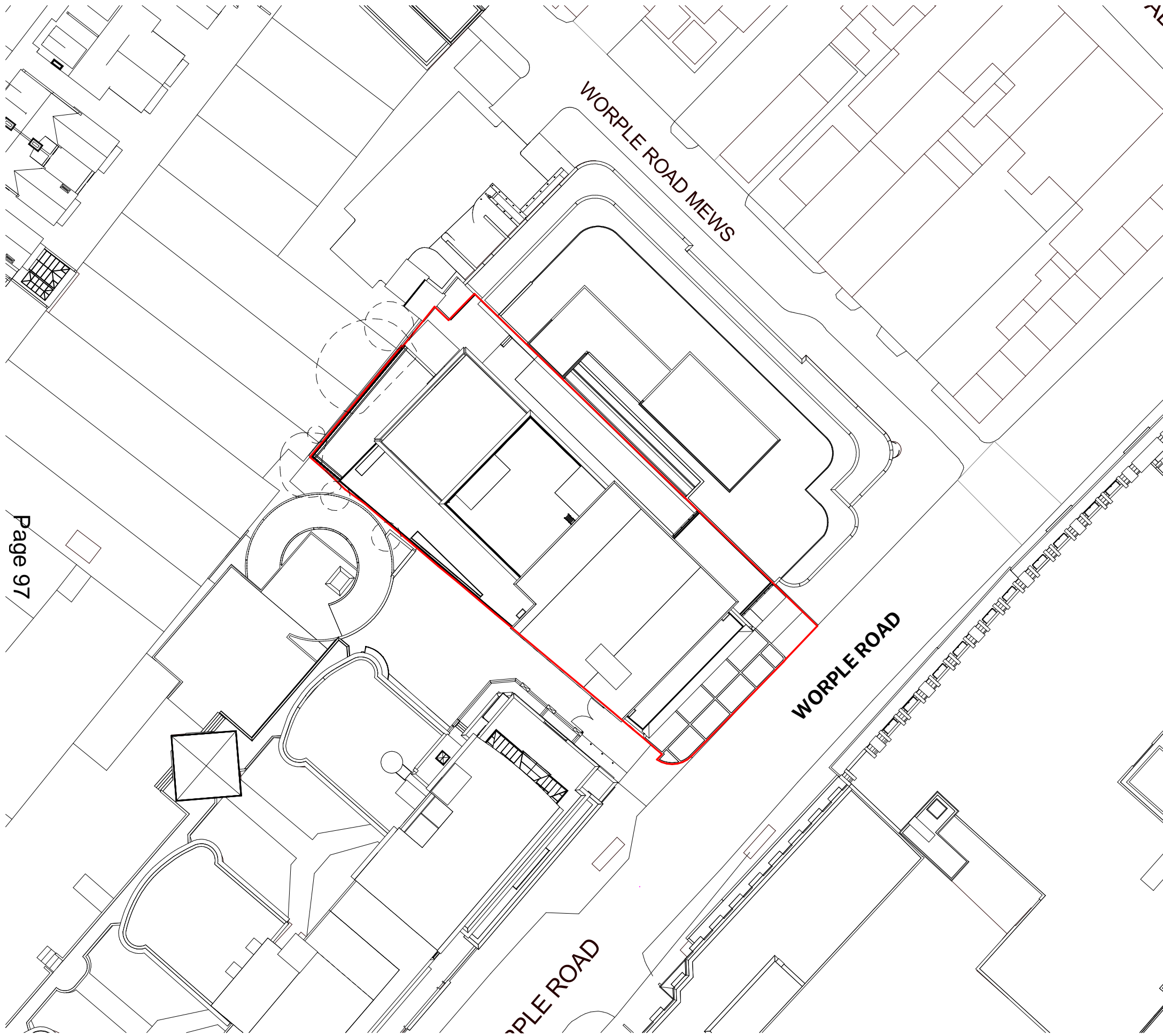
Site Plan - Existing

1 : 500



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| Project | | | | |
| 07758 - 19 Worple Road | | | | |
| Drawing Title | | | | |
| Site Plan - Existing | | | | |
| Project No. | Drawing No. | Status | Revision | Scale at A3 |
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| File Reference: 07758-SPACE-ZZ-XX-DR-A-90-0001 | | | | |

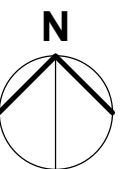
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| Revision | Description | Author | Checker | Date |
|----------|--|--------|---------|----------|
| P1 | First issue, issued for Board review | LC | PB | 02.07.20 |
| P2 | Red line boundary amended | LC | PB | 15.07.20 |
| P3 | Red line boundary amended to include pavement | LC | PB | 18.08.20 |
| P4 | Drawing amended to highlight extent of ownership | LC | PB | 26.08.20 |
| P5 | Design freeze issue | LC | PB | 15.09.20 |
| P6 | Draft Planning Submission | LC | PB | 23.09.20 |
| P7 | Planning Submission | LC | PB | 17.11.20 |
| P8 | Planning issue, responding to comments received from the Local Planning Authority. | AW | DW | 06.08.21 |

Application Boundary

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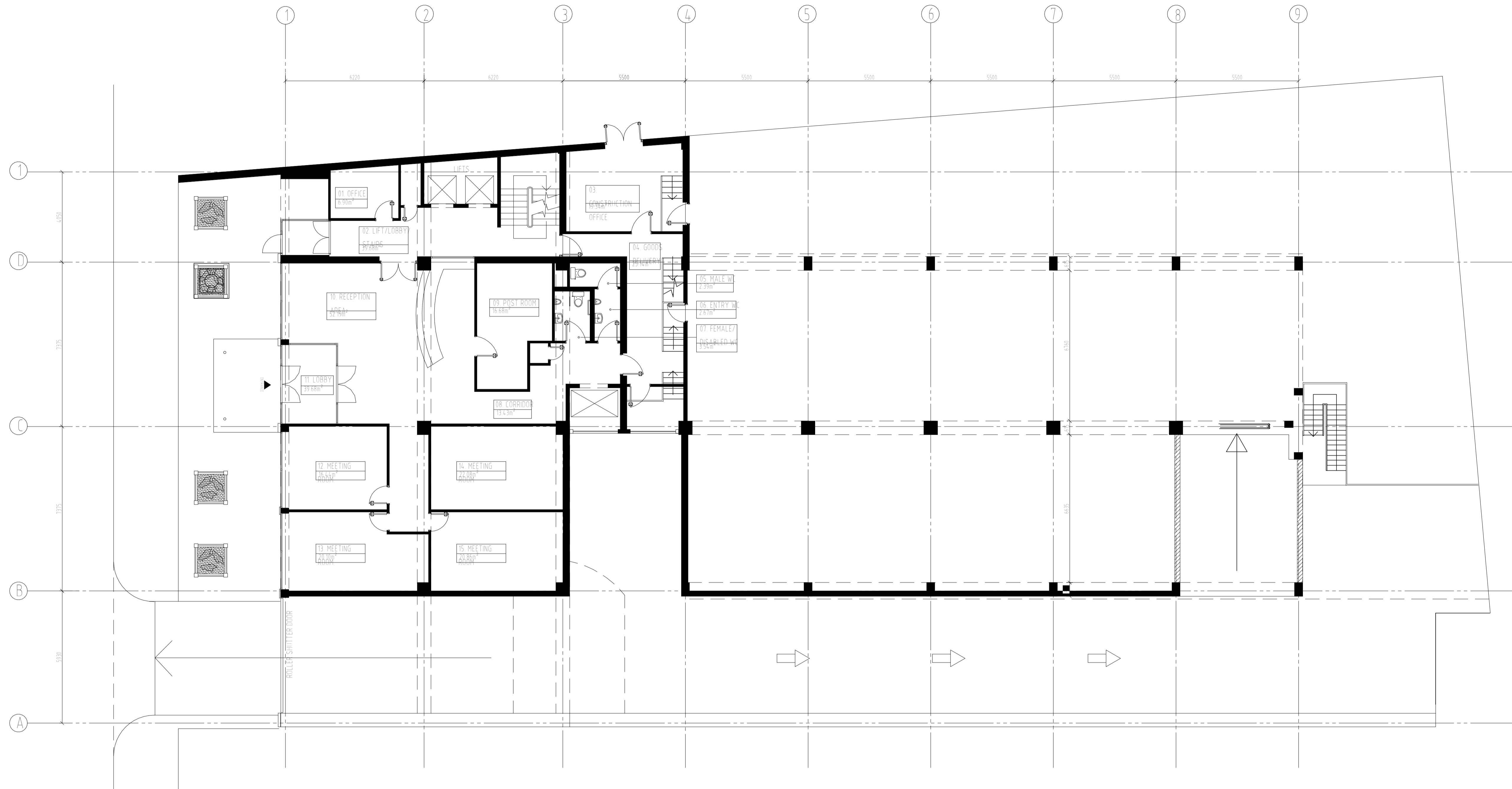
Site Plan - Proposed

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| Project | | | | |
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| Drawing Title | | | | |
| Site Plan - Proposed | | | | |
| Project No. | Drawing No. | Status | Revision | Scale at A3 |
| 07758 | ZZ-XX-DR-A-91-0001 | | P8 | As indicated |
| File Reference: 07758-SPACE-ZZ-XX-DR-A-91-0001 | | | | |

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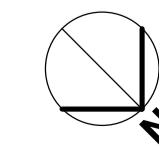
GA Plan - Existing - Level 00

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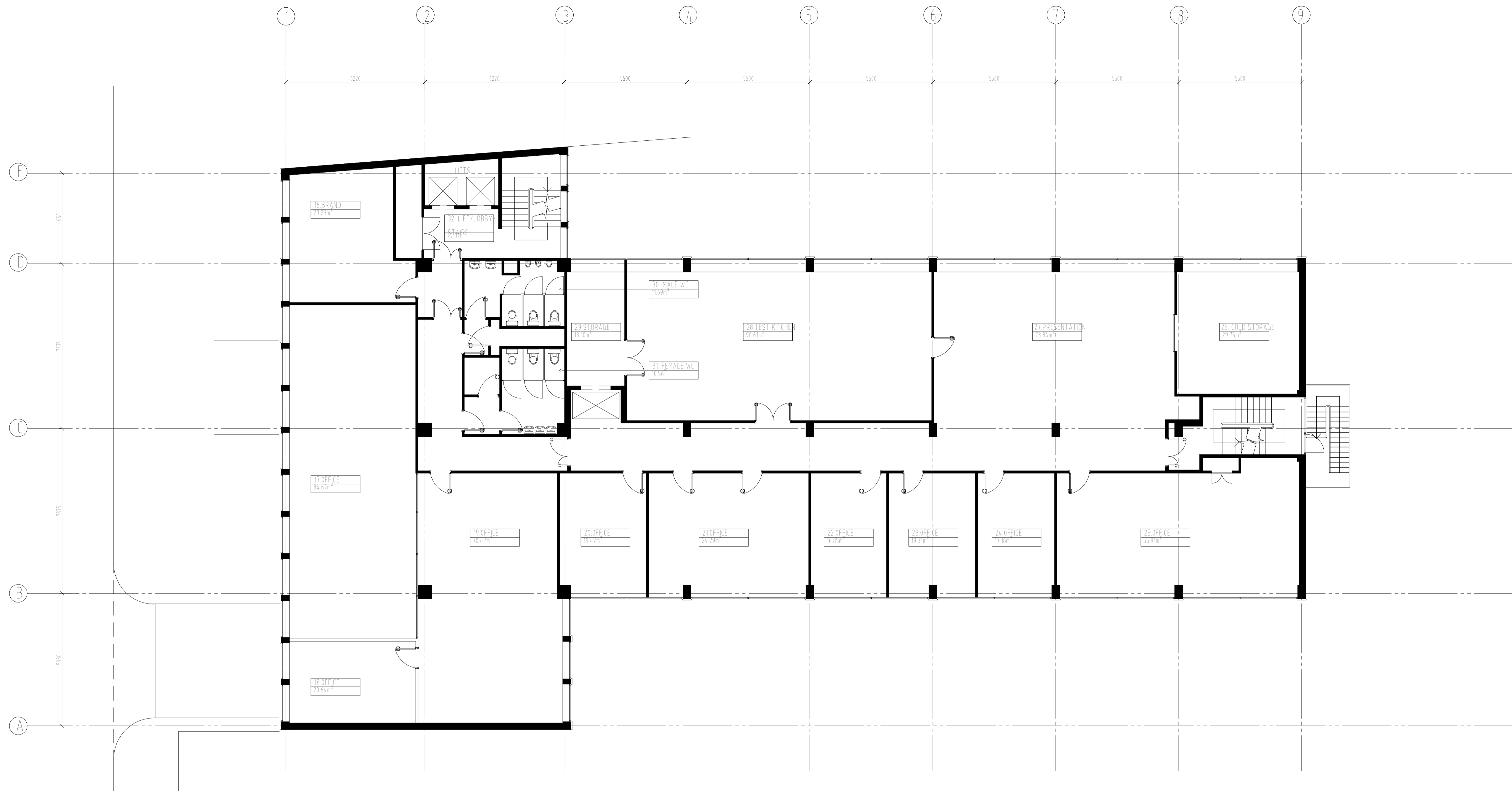
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| Project 07758 - 19 Worple Road | | | | |
| Drawing Title General Arrangement Plan - Existing - Level 00 | | | | |
| Project No. 07758 | Drawing No. ZZ-GF-DR-A-01-0051 | Status P1 | Revision 1 | Scale at A1 1:100 |
| File Reference: 07758-SPACE-ZZ-GF-DR-A-01-0051 | | | | |



| Revision | Description | Author | Checker | Date |
|----------|---------------------|--------|---------|----------|
| P1 | Planning Submission | LC | PB | 17.11.20 |



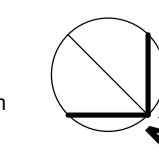
GA Plan - Existing - Level 01

1 : 100

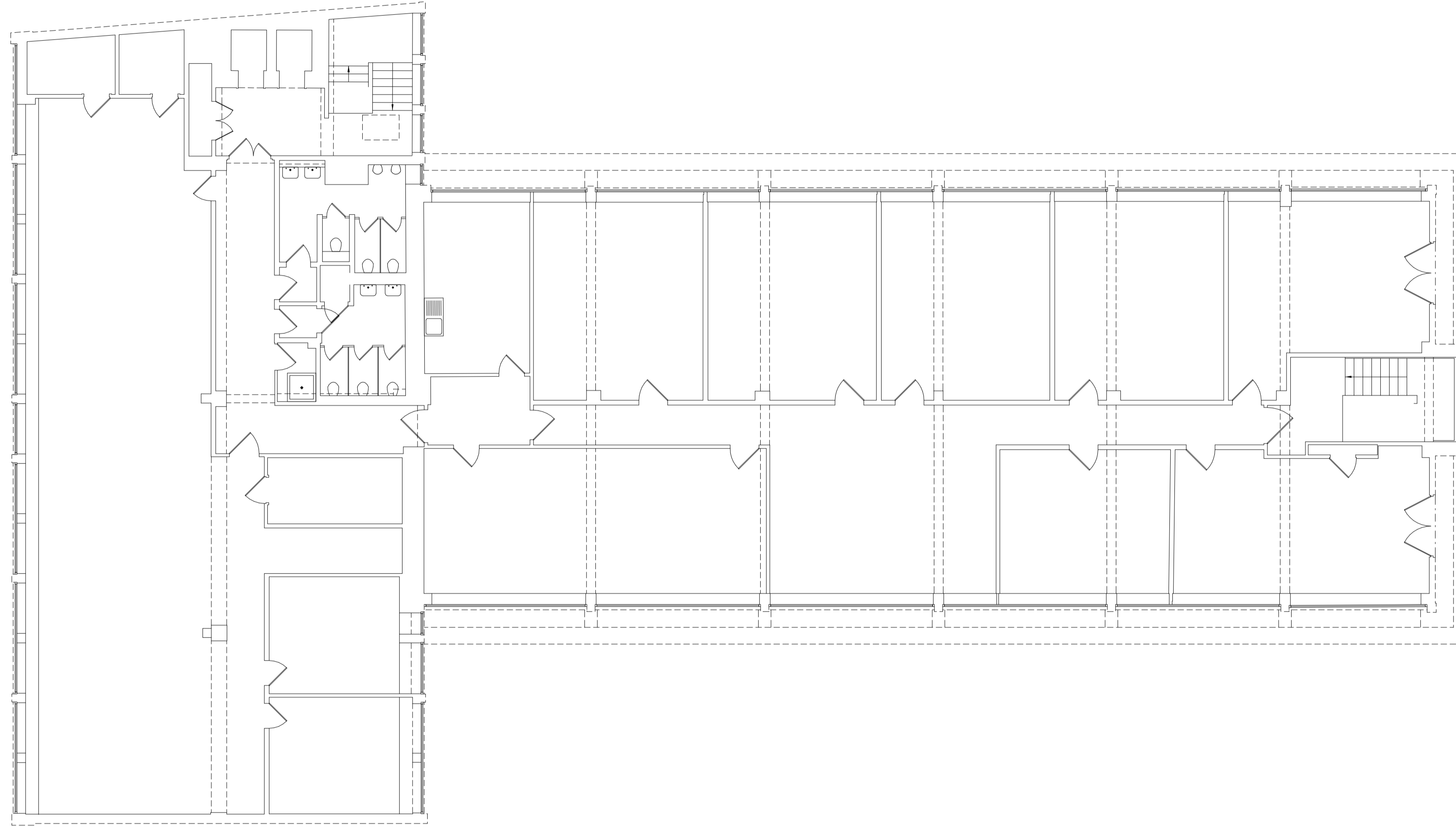
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| Project 07758 - 19 Worple Road | | | | |
| Drawing Title General Arrangement Plan - Existing - Level 01 | | | | |
| Project No. 07758 | Drawing No. ZZ-01-DR-A-01-0051 | Status P1 | Revision P1 | Scale at A1 1:100 |
| File Reference: 07758-SPACE-ZZ-01-DR-A-01-0051 | | | | |



| Revision | Description | Author | Checker | Date |
|----------|---------------------|--------|---------|----------|
| P1 | Planning Submission | LC | PB | 17.11.20 |



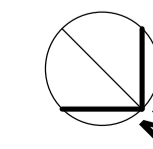
GA Plan - Existing - Level 06

1 : 100

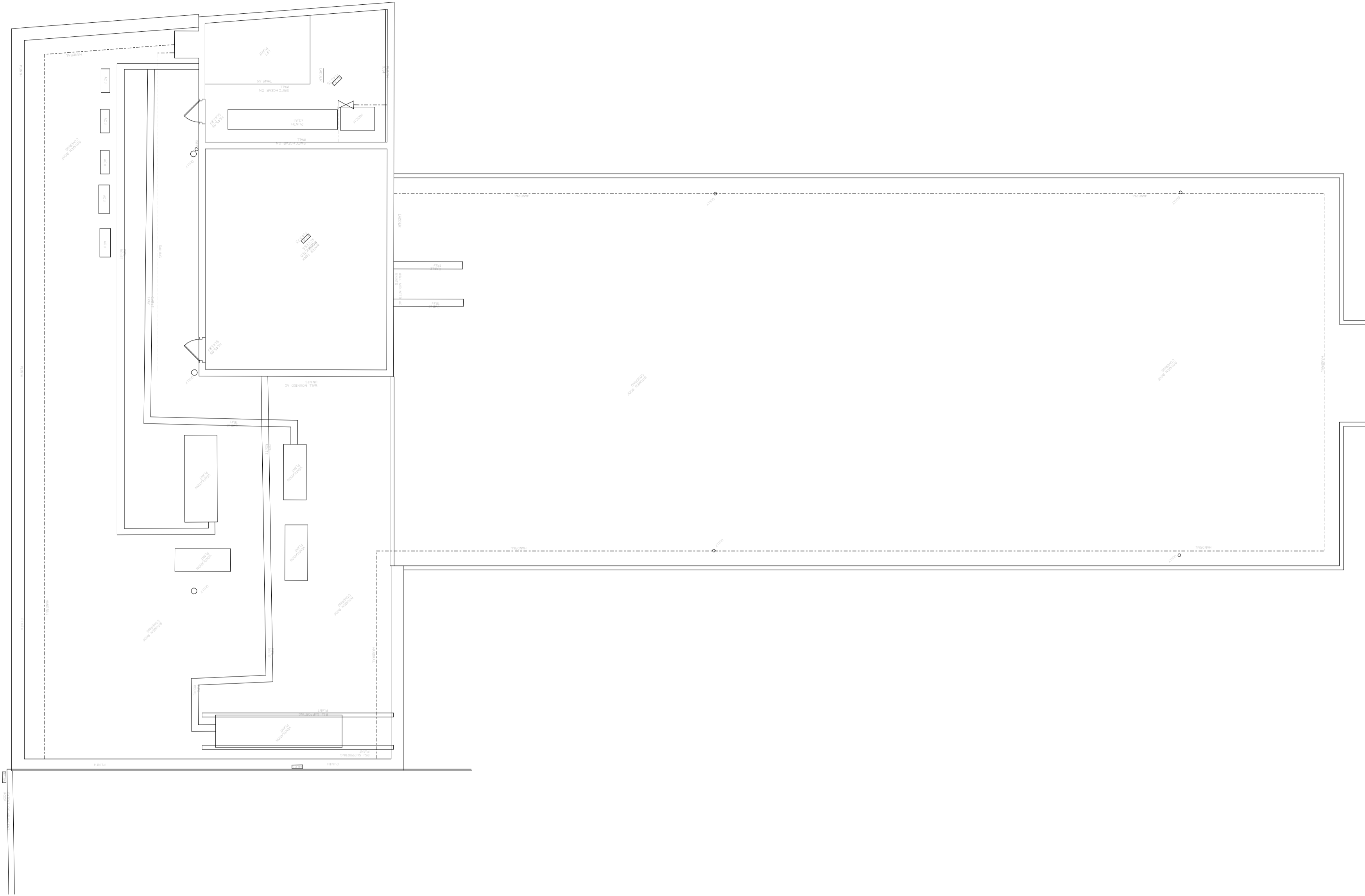
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| Project | | | | |
| 07758 - 19 Worple Road | | | | |
| Drawing Title | | | | |
| General Arrangement Plan - Existing - Level 06 | | | | |
| Project No. | Drawing No. | Status | Revision | Scale at A1 |
| 07758 | ZZ-06-DR-A-01-0051 | P1 | P1 | 1:100 |
| File Reference: 07758-SPACE-ZZ-06-DR-A-01-0051 | | | | |



| Revision | Description | Author | Checker | Date |
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| P1 | Planning Submission | LC | PB | 17.11.20 |



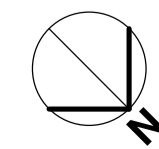
GA Plan - Existing - Roof Plan

1 : 100

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| Project 07758 - 19 Worple Road | | | | |
| Drawing Title General Arrangement Plan - Existing - Roof Plan | | | | |
| Project No. 07758 | Drawing No. ZZ-RF-DR-A-01-0051 | Status P1 | Revision P1 | Scale at A1 1:100 |
| File Reference: 07758-SPACE-ZZ-RF-DR-A-01-0051 | | | | |



| Revision | Description | Author | Checker | Date |
|----------|---------------------|--------|---------|----------|
| P1 | Planning Submission | LC | PB | 17.11.20 |

| Revision | Description | Author | Checker | Date |
|----------|---|--------|---------|----------|
| P1 | First issue of planning drawings, structural grid amended, ancillary accommodation/core developed | LC | PB | 29.06.20 |
| P2 | General design development, issued for Board review | LC | PB | 02.07.20 |
| P3 | Red line boundary amended | LC | PB | 15.07.20 |
| P4 | Design freeze issue | LC | PB | 07.08.20 |
| P5 | Final draft for comment prior to submission | LC | PB | 26.08.20 |
| P6 | General amendments made | LC | PB | 15.09.20 |
| P7 | Draft Planning Submission | LC | PB | 23.09.20 |
| P8 | Travellator repositioned, revised service and cycle access, revised escape strategy and basement layout | LC | PB | 11.11.20 |
| P9 | Planning Submission | LC | PB | 17.11.20 |
| P10 | Planning Submission - amended following further client comments prior to submission | LC | PB | 07.01.21 |
| P11 | Layout revised to suit natural smoke ventilation strategy. | AW | DW | 02.01.21 |
| P12 | Layout revised following consultation with client, consultants and specialist manufacturers. | AW | DW | 08.03.21 |
| P13 | GA Plan general revision including Lidl store layout amendments and changes to core area of offices. | GCJ | DW | 19.03.21 |
| P14 | Internal door and room references added. | AW | GCJ | 13.05.21 |
| P15 | Planning issue, responding to comments received from the Local Planning Authority. | AW | DW | 06.08.21 |
| P16 | Reissued for Planning | GCJ | DW | 19.08.21 |
| P17 | Revised following client comments received 06.09.21 | AW | DW | 14.09.21 |
| P18 | Paving to main entrance. Refer to Landscape proposals | AW | DW | 16.09.21 |



- External levels to rear as existing
- External retail plant
- Existing sub station
- Escape from office accommodation above. Door swing beyond 90 degrees for escape route.
- Full Sluice
- Lodge lifts (national spec)
- 2400mm Clear Sectional Door
- Glazed automatic door to cycle entrance
- Armco barriers to edge
- Power assisted wheel track
- Store travellator. Min 3300mm clear width in structure required.
- Edge protection bollards
- Line of security gate and cycle access.

General Arrangement
 Note:
 Coordination with structural, mechanical and electrical consultants ongoing.
 Floor areas subject to location of structural walls and columns - To be confirmed.
 Previous Planning Issue Rev P1



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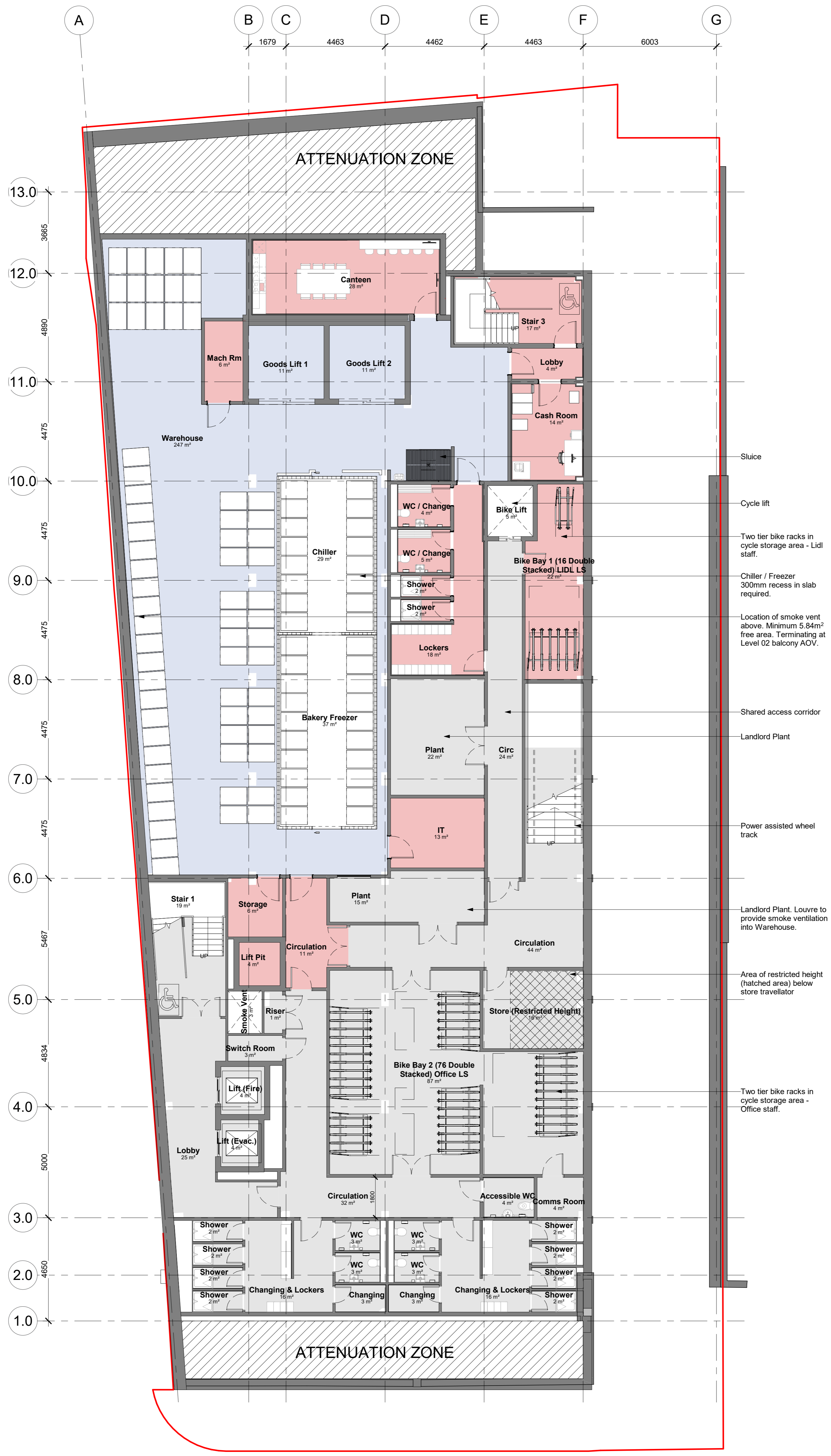
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| Project No. 07758 | Drawing No. ZZ-GF-DR-A-01-0001 | Status S3 | Revision P18 | Scale at A1 1:100 |
| File Reference: 07758-SPACE-ZZ-GF-DR-A-01-0001 | | | | |

WORPLE ROAD



| Revision | Description | Author | Checker | Date |
|----------|--|--------|---------|----------|
| P1 | First issue of planning drawings, structural grid amended, ancillary accommodation/core developed | LC | PB | 29.06.20 |
| P2 | General design development, issued for Board review | LC | PB | 02.07.20 |
| P3 | Red line boundary amended | LC | PB | 15.07.20 |
| P4 | Design freeze issue | LC | PB | 07.08.20 |
| P5 | Final draft for comment prior to submission | LC | PB | 26.08.20 |
| P6 | General amendments made | LC | PB | 15.09.20 |
| P7 | Draft Planning Submission | LC | PB | 23.09.20 |
| P8 | Travellator repositioned, revised service and cycle access, revised escape strategy and basement layout | LC | PB | 11.11.20 |
| P9 | Planning Submission | LC | PB | 17.11.20 |
| P10 | Layout revised to suit natural smoke ventilation strategy, as confirmed by DDA 12.05.21. | AW | DW | 02.01.21 |
| P11 | Layout revised following consultation with client, consultants and specialist manufacturers. | AW | DW | 08.03.21 |
| P12 | GA Plan general revision including Lidl store layout amendments and changes to core area of offices. | GCJ | DW | 19.03.21 |
| P13 | Internal door and room references added. Plant B-027 to be landlord space, as confirmed by DDA 12.05.21. | AW | GCJ | 13.05.21 |
| P14 | Planning issue, responding to comments received from the Local Planning Authority. | AW | DW | 06.08.21 |
| P15 | Revised for Planning | GCJ | DW | 19.08.21 |
| P16 | Revised following client comments received 06.09.21 | AW | DW | 14.09.21 |



Sluice
 Cycle lift
 Two tier bike racks in cycle storage area - Lidl staff.
 Chiller / Freezer 300mm recess in slab required.
 Location of smoke vent above. Minimum 5.84m² free area. Terminating at Level 02 balcony AOV.
 Shared access corridor
 Landlord Plant
 Power assisted wheel track
 Landlord Plant. Louvre to provide smoke ventilation into Warehouse.
 Area of restricted height (hatched area) below store traveller
 Two tier bike racks in cycle storage area - Office staff.

Retail Bike Numbers
 Sales 1169m2
 Long Stay 1169/175 = 7
 Short Stay (750/20) + (419/150) = 41
 Actual
 Long Stay 16
 Short Stay 8

Office Bike Numbers
 GIA 3882m2
 Long Stay 1 per 75m² = 52
 Short Stay 1 per 500m² = 8
 Actual
 Long Stay 76
 Short Stay 8

General Arrangement
 Note:
 Coordination with structural, mechanical and electrical consultants ongoing.
 Floor areas subject to location of structural walls and columns - To be confirmed.
 Previous Planning Issue Rev P1

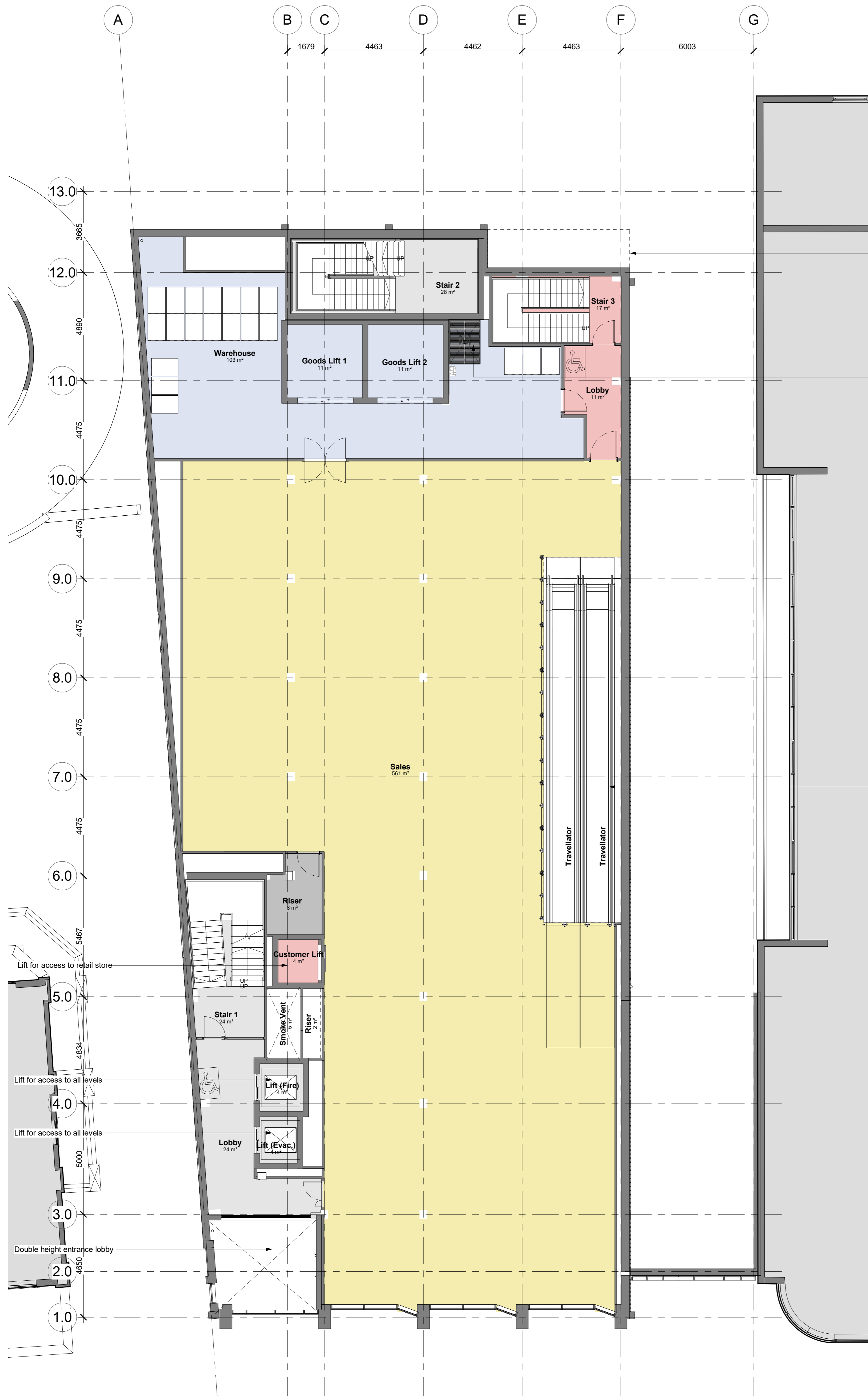
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| Project 07758 - 19 Worple Road | | | | |
| Drawing Title General Arrangement Plan - Proposed - Level -01 | | | | |
| Project No. 07758 | Drawing No. ZZ-B1-DR-A-01-0001 | Status S3 | Revision P16 | Scale at A1 1:100 |
| File Reference: 07758-SPACE-ZZ-B1-DR-A-01-0001 | | | | |



| Revision | Description | Author | Checker | Date |
|----------|---|--------|---------|----------|
| P1 | First issue of planning drawings, structural grid amended, ancillary accommodation/core developed | LC | PB | 29.06.20 |
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| P9 | Layout revised to suit natural smoke ventilation strategy. | AW | DW | 02.01.21 |
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| P11 | GA Plan general revision including Lidl store layout amendments and changes to core area of offices. | GCJ | DW | 19.03.21 |
| P12 | Internal door and room references added. | AW | GCJ | 13.05.21 |
| P13 | Planning issue, responding to comments received from the Local Planning Authority. | AW | DW | 06.08.21 |
| P14 | Revision P14 to match Conject | AW | GCJ | 06.08.21 |
| P15 | Revised for Planning | GCJ | DW | 19.08.21 |
| P16 | Revised following client comments received 06.09.21 | AW | DW | 14.09.21 |

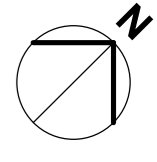


Cantilevered floor over.

Full sluice

Store travellator. Min 3300mm clear width in structure required.

General Arrangement
 Note:
 Coordination with structural, mechanical and electrical consultants ongoing.
 Floor areas subject to location of structural walls and columns - To be confirmed.
 Previous Planning Issue Rev P1



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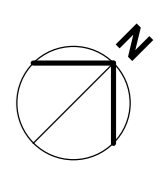
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| Project 07758 - 19 Worple Road | | | | |
| Drawing Title General Arrangement Plan - Proposed - Level 01 | | | | |
| Project No. 07758 | Drawing No. ZZ-01-DR-A-01-0001 | Status S3 | Revision P16 | Scale at A1 1:100 |
| File Reference: 07758-SPACE-ZZ-01-DR-A-01-0001 | | | | |



| Revision | Description | Author | Checker | Date |
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| P9 | Internal door and room references added. | AW | GCJ | 13.05.21 |
| P10 | Planning issue, responding to comments received from the Local Planning Authority. | AW | DW | 06.08.21 |
| P11 | Reissued for Planning | GCJ | DW | 19.08.21 |
| P12 | Revised following client comments received 06.09.21 | AW | DW | 14.09.21 |



General Arrangement
 Note:
 Coordination with structural, mechanical and electrical consultants ongoing.
 Floor areas subject to location of structural walls and columns - To be confirmed.
 Previous Planning Issue Rev P1



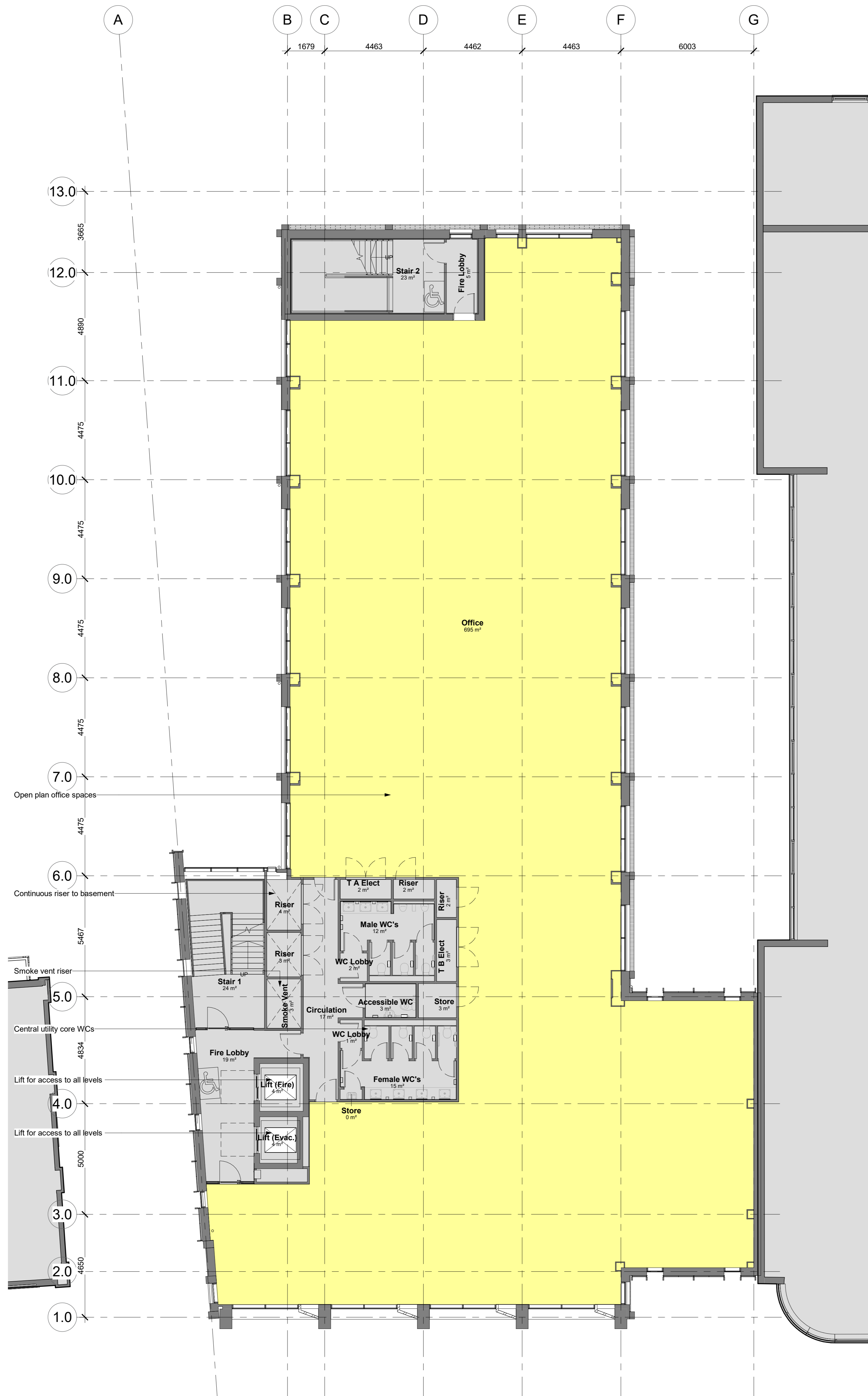
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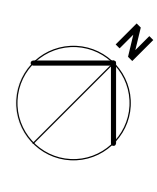
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| Project 07758 - 19 Worple Road | | | | |
| Drawing Title General Arrangement Plan - Proposed - Level 02 | | | | |
| Project No. 07758 | Drawing No. ZZ-02-DR-A-01-0001 | Status S3 | Revision P12 | Scale at A1 1:100 |
| File Reference: 07758-SPACE-ZZ-02-DR-A-01-0001 | | | | |



| Revision | Description | Author | Checker | Date |
|----------|--|--------|---------|----------|
| P1 | First issue of planning drawings, structural grid amended, ancillary accommodation/core developed | LC | PB | 29.06.20 |
| P2 | General design development, issued for Board review | LC | PB | 02.07.20 |
| P3 | Design freeze issue | LC | PB | 07.08.20 |
| P4 | Final draft for comment prior to submission | LC | PB | 26.08.20 |
| P5 | General amendments made | LC | PB | 15.09.20 |
| P6 | Draft Planning Submission | LC | PB | 23.09.20 |
| P7 | Planning Submission | LC | PB | 17.11.20 |
| P8 | GA Plan general revision including Lidl store layout amendments and changes to core area of offices. | GCJ | DW | 19.03.21 |
| P9 | Internal door and room references added. | AW | GCJ | 13.05.21 |
| P10 | Planning issue, responding to comments received from the Local Planning Authority. | AW | DW | 06.08.21 |
| P11 | Reissued for Planning | GCJ | DW | 19.08.21 |
| P12 | Revised following client comments received 06.09.21 | AW | DW | 14.09.21 |



General Arrangement
 Note:
 Coordination with structural, mechanical and electrical consultants ongoing.
 Floor areas subject to location of structural walls and columns - To be confirmed.
 Previous Planning Issue Rev P1



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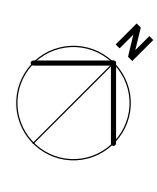
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|--|--|---------------------|------------------------|-----------------------------|
| Project 07758 - 19 Worple Road | | | | |
| Drawing Title General Arrangement Plan - Proposed - Level 03 | | | | |
| Project No. 07758 | Drawing No. ZZ-03-DR-A-01-0001 | Status S3 | Revision P12 | Scale at A1 1:100 |
| File Reference: 07758-SPACE-ZZ-03-DR-A-01-0001 | | | | |



| Revision | Description | Author | Checker | Date |
|----------|--|--------|---------|----------|
| P1 | First issue of planning drawings, structural grid amended, ancillary accommodation/core developed | LC | PB | 29.06.20 |
| P2 | General design development, issued for Board review | LC | PB | 02.07.20 |
| P3 | Design freeze issue | LC | PB | 07.08.20 |
| P4 | Final draft for comment prior to submission | LC | PB | 26.08.20 |
| P5 | General amendments made | LC | PB | 15.09.20 |
| P6 | Draft Planning Submission | LC | PB | 23.09.20 |
| P7 | Planning Submission | LC | PB | 17.11.20 |
| P8 | GA Plan general revision including Lidl store layout amendments and changes to core area of offices. | GCJ | DW | 19.03.21 |
| P9 | Internal door and room references added. | AW | GCJ | 13.05.21 |
| P10 | Planning issue, responding to comments received from the Local Planning Authority. | AW | DW | 06.08.21 |
| P11 | Reissued for Planning | GCJ | DW | 19.08.21 |
| P12 | Revised following client comments received 06.09.21 | AW | DW | 14.09.21 |



General Arrangement
 Note:
 Coordination with structural, mechanical and electrical consultants ongoing.
 Floor areas subject to location of structural walls and columns - To be confirmed.
 Previous Planning Issue Rev P1



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|--|--|---------------------|------------------------|-----------------------------|
| Project 07758 - 19 Worple Road | | | | |
| Drawing Title General Arrangement Plan - Proposed - Level 04 | | | | |
| Project No. 07758 | Drawing No. ZZ-04-DR-A-01-0001 | Status S3 | Revision P12 | Scale at A1 1:100 |
| File Reference: 07758-SPACE-ZZ-04-DR-A-01-0001 | | | | |



| Revision | Description | Author | Checker | Date |
|----------|--|--------|---------|----------|
| P1 | First issue of planning drawings, structural grid amended, ancillary accommodation/core developed | LC | PB | 29.06.20 |
| P2 | General design development, issued for Board review | LC | PB | 02.07.20 |
| P3 | Design freeze issue | LC | PB | 07.08.20 |
| P4 | Final draft for comment prior to submission | LC | PB | 26.08.20 |
| P5 | General amendments made | LC | PB | 15.09.20 |
| P6 | Draft Planning Submission | LC | PB | 23.09.20 |
| P7 | Planning Submission | LC | PB | 17.11.20 |
| P8 | GA Plan general revision including Lidl store layout amendments and changes to core area of offices. | GCJ | DW | 19.03.21 |
| P9 | Internal door and room references added. | AW | GCJ | 13.05.21 |
| P10 | Planning issue, responding to comments received from the Local Planning Authority. | AW | DW | 06.08.21 |
| P11 | Reissued for Planning | GCJ | DW | 19.08.21 |
| P12 | Revised following client comments received 06.09.21 | AW | DW | 14.09.21 |



General Arrangement
 Note:
 Coordination with structural, mechanical and electrical consultants ongoing.
 Floor areas subject to location of structural walls and columns - To be confirmed.
 Previous Planning Issue Rev P1

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|--|--|---------------------|------------------------|-----------------------------|
| Project 07758 - 19 Worpole Road | | | | |
| Drawing Title General Arrangement Plan - Proposed - Level 05 | | | | |
| Project No. 07758 | Drawing No. ZZ-05-DR-A-01-0001 | Status S3 | Revision P12 | Scale at A1 1:100 |
| File Reference: 07758-SPACE-ZZ-05-DR-A-01-0001 | | | | |



| Revision | Description | Author | Checker | Date |
|----------|--|--------|---------|----------|
| P1 | First issue of planning drawings, structural grid amended, ancillary accommodation/core developed | LC | PB | 29.06.20 |
| P2 | General design development, issued for Board review | LC | PB | 02.07.20 |
| P3 | Design freeze issue | LC | PB | 07.08.20 |
| P4 | Final draft for comment prior to submission | LC | PB | 26.08.20 |
| P5 | General amendments made | LC | PB | 15.09.20 |
| P6 | Draft Planning Submission | LC | PB | 23.09.20 |
| P7 | Planning Submission | LC | PB | 17.11.20 |
| P8 | GA Plan general revision including Lidl store layout amendments and changes to core area of offices. | GCJ | DW | 19.03.21 |
| P9 | Internal door and room references added. | AW | GCJ | 13.05.21 |
| P10 | Planning issue, responding to comments received from the Local Planning Authority. | AW | DW | 06.08.21 |
| P11 | Reissued for Planning | GCJ | DW | 19.08.21 |
| P12 | Revised following client comments received 06.09.21 | AW | DW | 14.09.21 |



General Arrangement
 Note:
 Coordination with structural, mechanical and electrical consultants ongoing.
 Floor areas subject to location of structural walls and columns - To be confirmed.
 Previous Planning Issue Rev P1

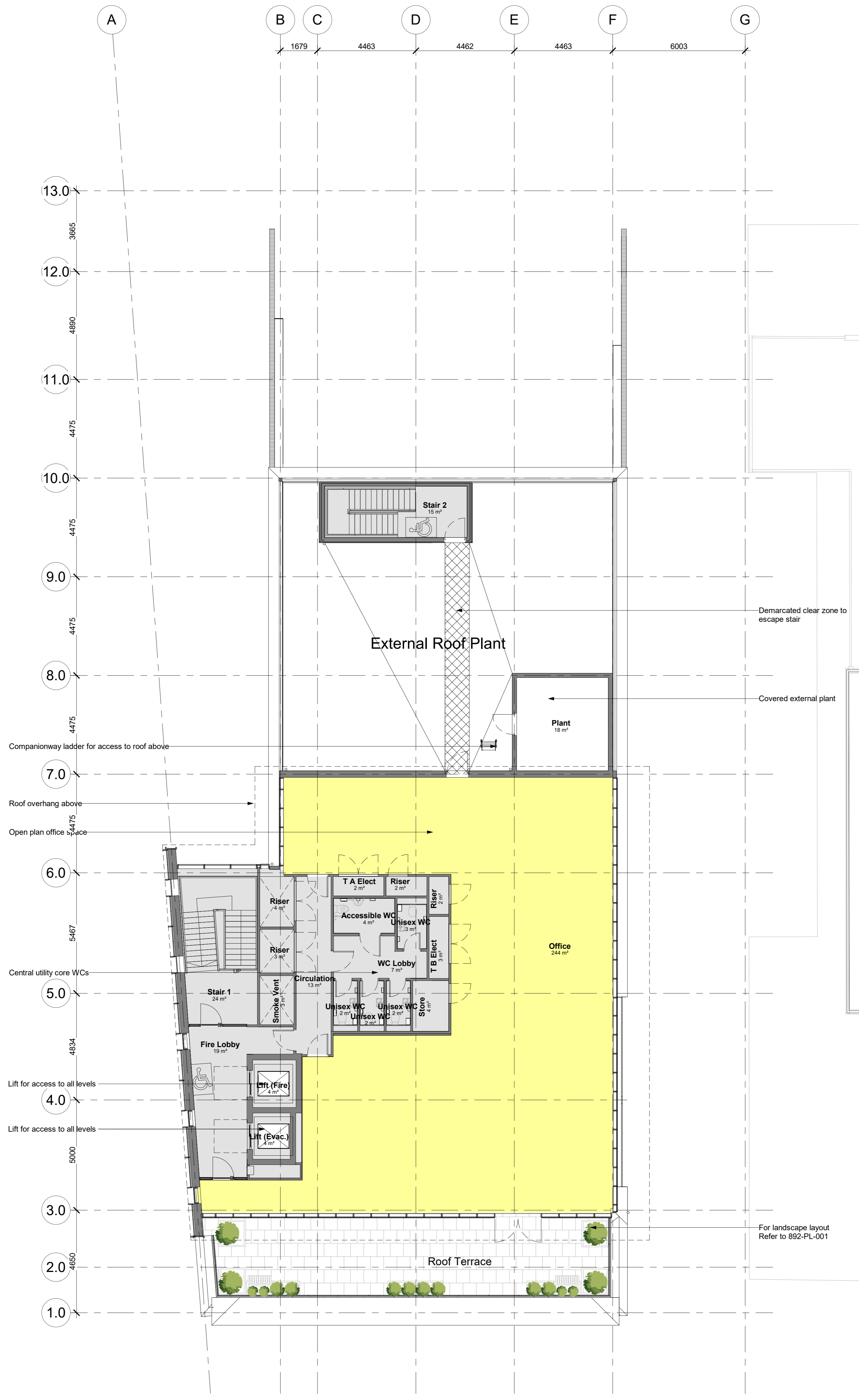
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|--|--|---------------------|------------------------|-----------------------------|
| Project 07758 - 19 Worple Road | | | | |
| Drawing Title General Arrangement Plan - Proposed - Level 06 | | | | |
| Project No. 07758 | Drawing No. ZZ-06-DR-A-01-0001 | Status S3 | Revision P12 | Scale at A1 1:100 |
| File Reference: 07758-SPACE-ZZ-06-DR-A-01-0001 | | | | |



| Revision | Description | Author | Checker | Date |
|----------|--|--------|---------|----------|
| P1 | First issue of planning drawings, structural grid amended, ancillary accommodation/core developed | LC | PB | 29.06.20 |
| P2 | General design development, issued for Board review | LC | PB | 02.07.20 |
| P3 | Design freeze issue | LC | PB | 07.08.20 |
| P4 | Final draft for comment prior to submission | LC | PB | 26.08.20 |
| P5 | General amendments made | LC | PB | 15.09.20 |
| P6 | Draft Planning Submission | LC | PB | 23.09.20 |
| P7 | Planning Submission | LC | PB | 17.11.20 |
| P8 | GA Plan general revision including Lidl store layout amendments and changes to core area of offices. | GCJ | DW | 19.03.21 |
| P9 | Internal door and room references added. | AW | GCJ | 13.05.21 |
| P10 | Planning issue, responding to comments received from the Local Planning Authority. | AW | DW | 06.08.21 |
| P11 | Reissued for Planning | GCJ | DW | 19.08.21 |
| P12 | Revised following client comments received 06.09.21 | AW | DW | 14.09.21 |



General Arrangement
 Note:
 Coordination with structural, mechanical and electrical consultants ongoing.
 Floor areas subject to location of structural walls and columns - To be confirmed.
 Previous Planning Issue Rev P1

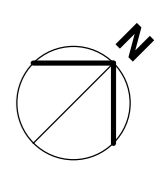
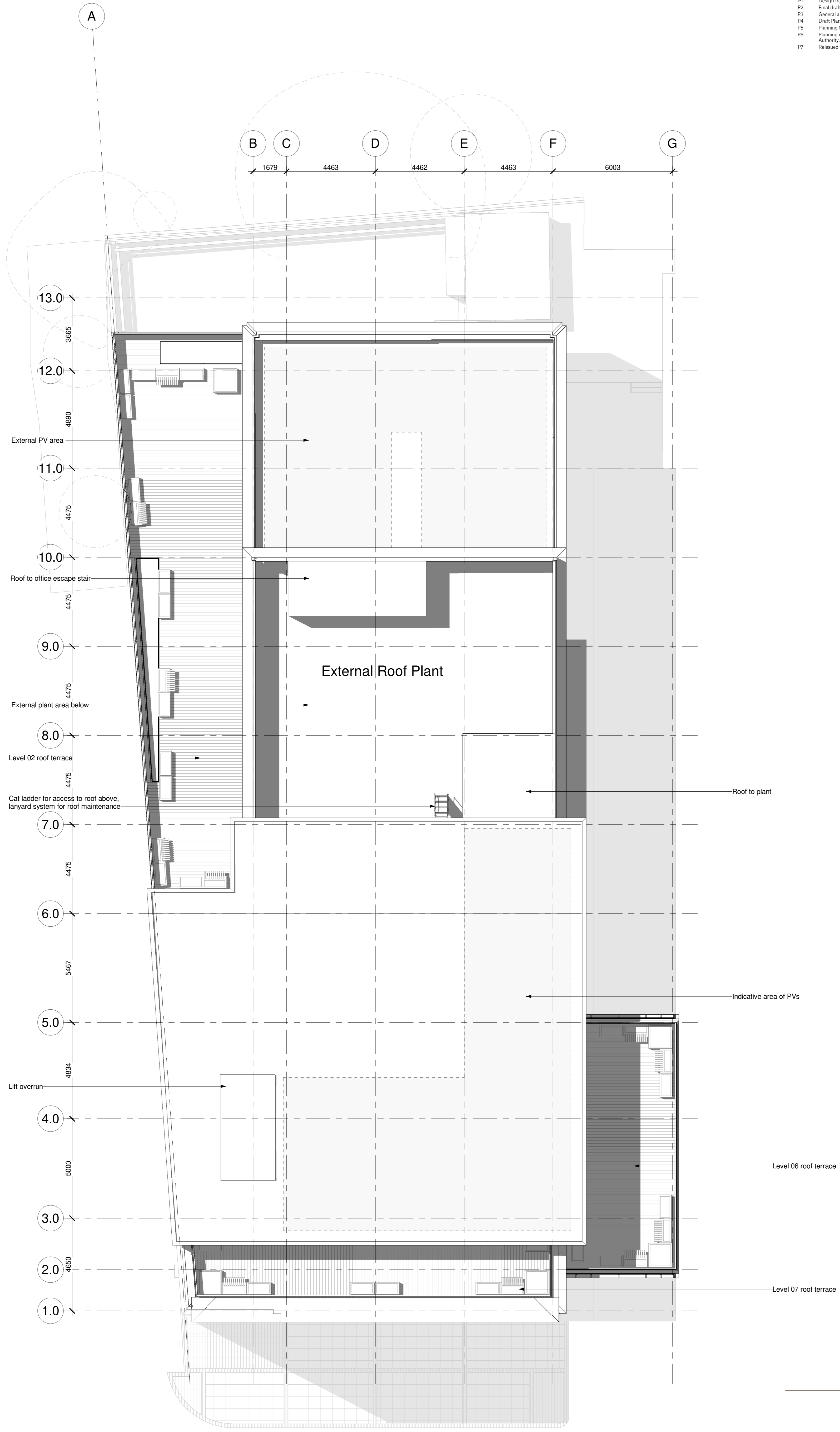
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|--|--|---------------------|------------------------|-----------------------------|
| Project 07758 - 19 Worple Road | | | | |
| Drawing Title General Arrangement Plan - Proposed - Level 07 | | | | |
| Project No. 07758 | Drawing No. ZZ-07-DR-A-01-0001 | Status S3 | Revision P12 | Scale at A1 1:100 |
| File Reference: 07758-SPACE-ZZ-07-DR-A-01-0001 | | | | |



| Revision | Description | Author | Checker | Date |
|----------|--|--------|---------|----------|
| P1 | Design freeze issue | LC | PB | 07.08.20 |
| P2 | Final draft for comment prior to submission | LC | PB | 26.08.20 |
| P3 | General amendments made | LC | PB | 15.09.20 |
| P4 | Draft Planning Submission | LC | PB | 23.09.20 |
| P5 | Planning Submission | LC | PB | 17.11.20 |
| P6 | Planning issue, responding to comments received from the Local Planning Authority. | AW | DW | 06.08.21 |
| P7 | Reissued for Planning | GCJ | DW | 19.08.21 |

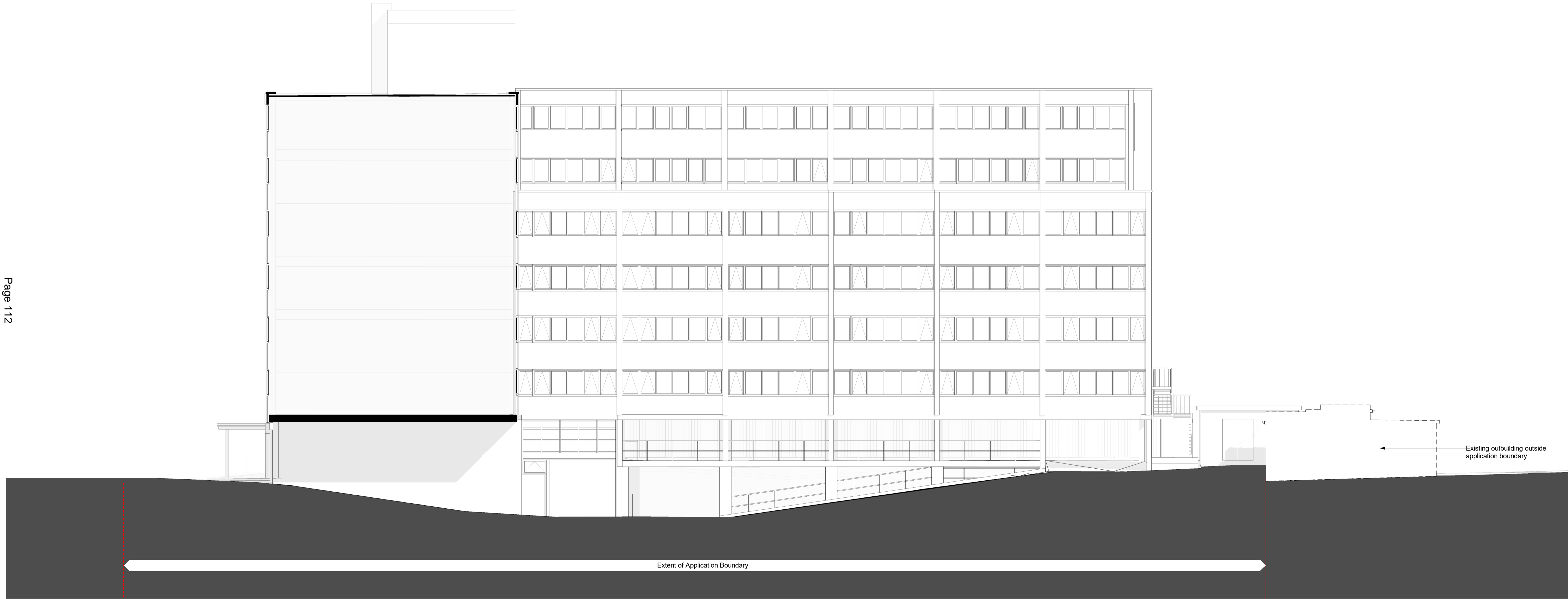


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|---|--|---------------------|-----------------------|-----------------------------|
| Project 07758 - 19 Worple Road | | | | |
| Drawing Title General Arrangement Plan - Proposed - Roof Plan | | | | |
| Project No. 07758 | Drawing No. ZZ-RF-DR-A-01-0001 | Status S3 | Revision P7 | Scale at A1 1:100 |
| File Reference: 07758-SPACE-ZZ-RF-DR-A-01-0001 | | | | |





General Arrangement Elevation - Existing - NE

1 : 100

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|---|---------------------------|--------|-----------|--------------|
| Project | | | | |
| 07758 - 19 Worple Road | | | | |
| Drawing Title | | | | |
| General Arrangement Elevation - Existing - NE | | | | |
| Project No. | Drawing No. | Status | Revision | Scale at A1 |
| 07758 | ZZ-XX-DR-A-02-0054 | | P1 | 1:100 |
| File Reference: 07758-SPACE-ZZ-XX-DR-A-02-0054 | | | | |





General Arrangement Elevation - Existing - NW

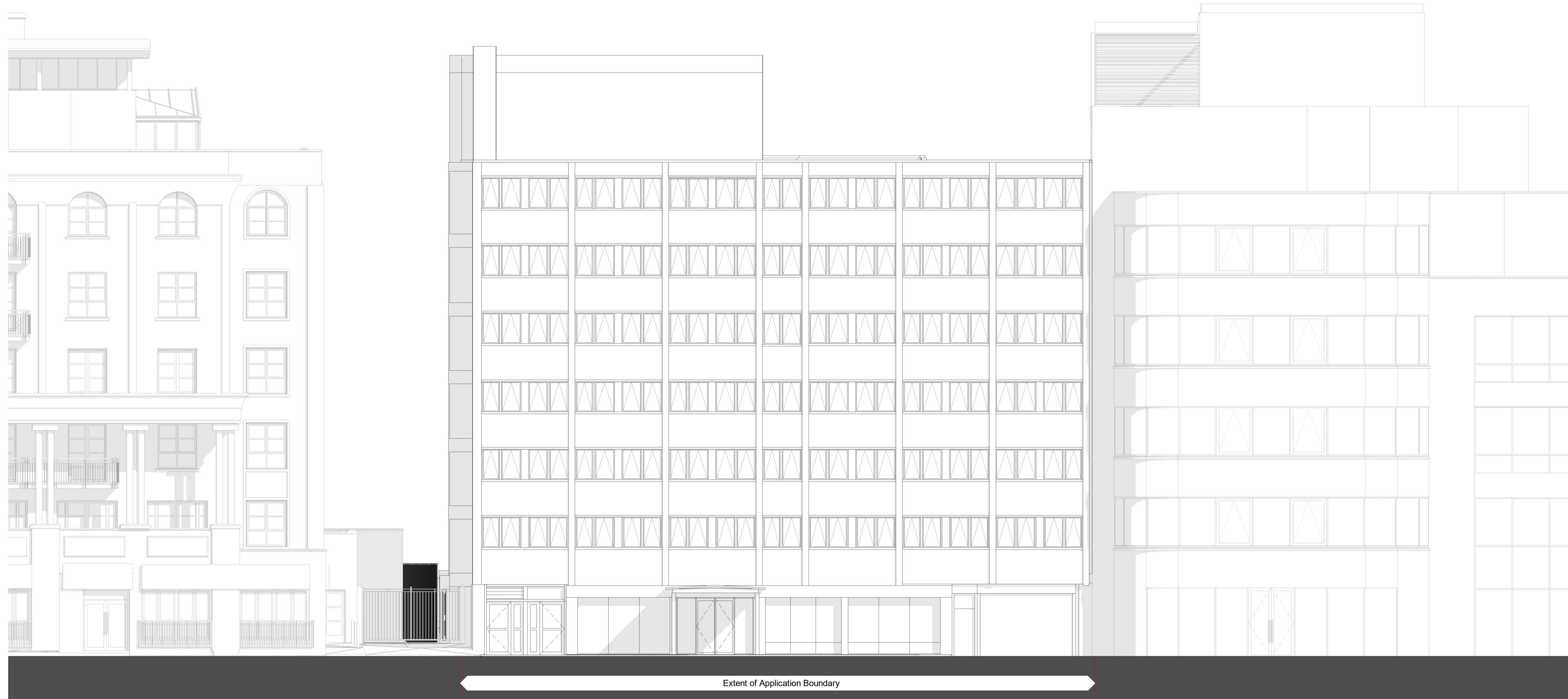
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|---|---------------------------|-----------|-----------|--------------|
| Project | | | | |
| 07758 - 19 Worple Road | | | | |
| Drawing Title | | | | |
| General Arrangement Elevation - Existing - NW | | | | |
| Project No. | Drawing No. | Status | Revision | Scale at A1 |
| 07758 | ZZ-XX-DR-A-02-0053 | P1 | P1 | 1:100 |
| File Reference: 07758-SPACE-ZZ-XX-DR-A-02-0053 | | | | |





GA Elevation - Existing - SE (Worple Road)

1 : 100

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| | | | | |
|--|---------------------------|--------|-----------|----------------|
| Project | | | | |
| 07758 - 19 Worple Road | | | | |
| Drawing Title | | | | |
| General Arrangement Elevation - Existing - SE (Worple Road) | | | | |
| Project No. | Drawing No. | Status | Revision | Scale at A1 |
| 07758 | ZZ-XX-DR-A-02-0051 | | P1 | 1 : 100 |
| File Reference: 07758-SPACE-ZZ-XX-DR-A-02-0051 | | | | |





General Arrangement Elevation - Existing - SW

1 : 100

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|---|---------------------------|--------|-----------|--------------|
| Project | | | | |
| 07758 - 19 Worple Road | | | | |
| Drawing Title | | | | |
| General Arrangement Elevation - Existing - SW | | | | |
| Project No. | Drawing No. | Status | Revision | Scale at A1 |
| 07758 | ZZ-XX-DR-A-02-0052 | | P1 | 1:100 |
| File Reference: 07758-SPACE-ZZ-XX-DR-A-02-0052 | | | | |



| Revision | Description | Author | Checker | Date |
|----------|--|--------|---------|----------|
| P1 | First issue of planning drawings | LC | PB | 29.06.20 |
| P2 | General design development, issued for Board review | LC | PB | 02.07.20 |
| P3 | Design freeze issue | LC | PB | 07.08.20 |
| P4 | Final draft for comment prior to submission | LC | PB | 26.08.20 |
| P5 | General amendments made | LC | PB | 15.09.20 |
| P6 | Draft Planning Submission | LC | PB | 23.09.20 |
| P7 | Planning Submission | LC | PB | 17.11.20 |
| P8 | Planning issue, responding to comments received from the Local Planning Authority. | AW | DW | 06.08.21 |
| P9 | Reissued for Planning | GCI | DW | 19.08.21 |
| P10 | Sectional and Delivery door colour revised to grey, at client request. | AW | DW | 22.09.21 |



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| | | | | |
|---|---------------------------|--------|------------|--------------|
| Project | | | | |
| 07758 - 19 Worple Road | | | | |
| Drawing Title | | | | |
| General Arrangement Elevation - Proposed - NE | | | | |
| Project No. | Drawing No. | Status | Revision | Scale at A1 |
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| File Reference: 07758-SPACE-ZZ-XX-DR-A-02-0004 | | | | |



| Revision | Description | Author | Checker | Date |
|----------|--|--------|---------|----------|
| P1 | First issue of planning drawings | LC | PB | 29.06.20 |
| P2 | General design development, issued for Board review | LC | PB | 02.07.20 |
| P3 | Design freeze issue | LC | PB | 07.08.20 |
| P4 | Final draft for comment prior to submission | LC | PB | 26.08.20 |
| P5 | General amendments made | LC | PB | 15.09.20 |
| P6 | Draft Planning Submission | LC | PB | 23.09.20 |
| P7 | Planning Submission | LC | PB | 17.11.20 |
| P8 | Planning issue, responding to comments received from the Local Planning Authority. | AW | DW | 06.08.21 |
| P9 | Reissued for Planning | GCI | DW | 19.08.21 |



(unrun to lift over dimension) / 00862
 Page 117

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|---|--|---------------------|----------------------|-----------------------------|
| Project 07758 - 19 Worple Road | | | | |
| Drawing Title General Arrangement Elevation - Proposed - NW | | | | |
| Project No. 07758 | Drawing No. ZZ-XX-DR-A-02-0003 | Status P9 | Revision 1 | Scale at A1 1:100 |
| File Reference: 07758-SPACE-ZZ-XX-DR-A-02-0003 | | | | |



| Revision | Description | Author | Checker | Date |
|----------|---|--------|---------|----------|
| P1 | First issue of planning drawings | LC | PB | 29.06.20 |
| P2 | General design development, issued for Board review | LC | PB | 02.07.20 |
| P3 | Design freeze issue | LC | PB | 07.08.20 |
| P4 | Final draft for comment prior to submission | LC | PB | 26.08.20 |
| P5 | General amendments made | LC | PB | 15.09.20 |
| P6 | Draft Planning Submission | LC | PB | 23.09.20 |
| P7 | Planning Submission | LC | PB | 17.11.20 |
| P8 | Planning issue, responding to comments received from the Local Planning Authority. | AW | DW | 06.08.21 |
| P9 | Reissued for Planning | GCI | DW | 19.08.21 |
| P10 | Lidl Store curtain glazing frames revised from RAL 5010 to RAL 7024, in accordance with client request. | AW | DW | 08.09.21 |



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Front Elevation
1 : 100



Street Scene
1 : 500

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|--|--|----------------------|---------------------------------|------------------------------------|
| Project 07758 - 19 Worples Road | | | | |
| Drawing Title General Arrangement Elevation - Proposed - SE (Worples Road) | | | | |
| Project No. 07758 | Drawing No. ZZ-XX-DR-A-02-0001 | Status P10 | Revision As indicated | Scale at A1 As indicated |
| File Reference: 07758-SPACE-ZZ-XX-DR-A-02-0001 | | | | |



| Revision | Description | Author | Checker | Date |
|----------|--|--------|---------|----------|
| P1 | First issue | LC | PB | 10.06.20 |
| P2 | General design development, issued for Board review | LC | PB | 02.07.20 |
| P3 | Design freeze issue | LC | PB | 07.08.20 |
| P4 | Final draft for comment prior to submission | LC | PB | 26.08.20 |
| P5 | General amendments made | LC | PB | 15.09.20 |
| P6 | Draft Planning Submission | LC | PB | 23.09.20 |
| P7 | Planning Submission | LC | PB | 17.11.20 |
| P8 | Planning issue, responding to comments received from the Local Planning Authority. | AW | DW | 06.08.21 |
| P9 | Reissued for Planning | GCI | DW | 19.08.21 |



06862 Page 119

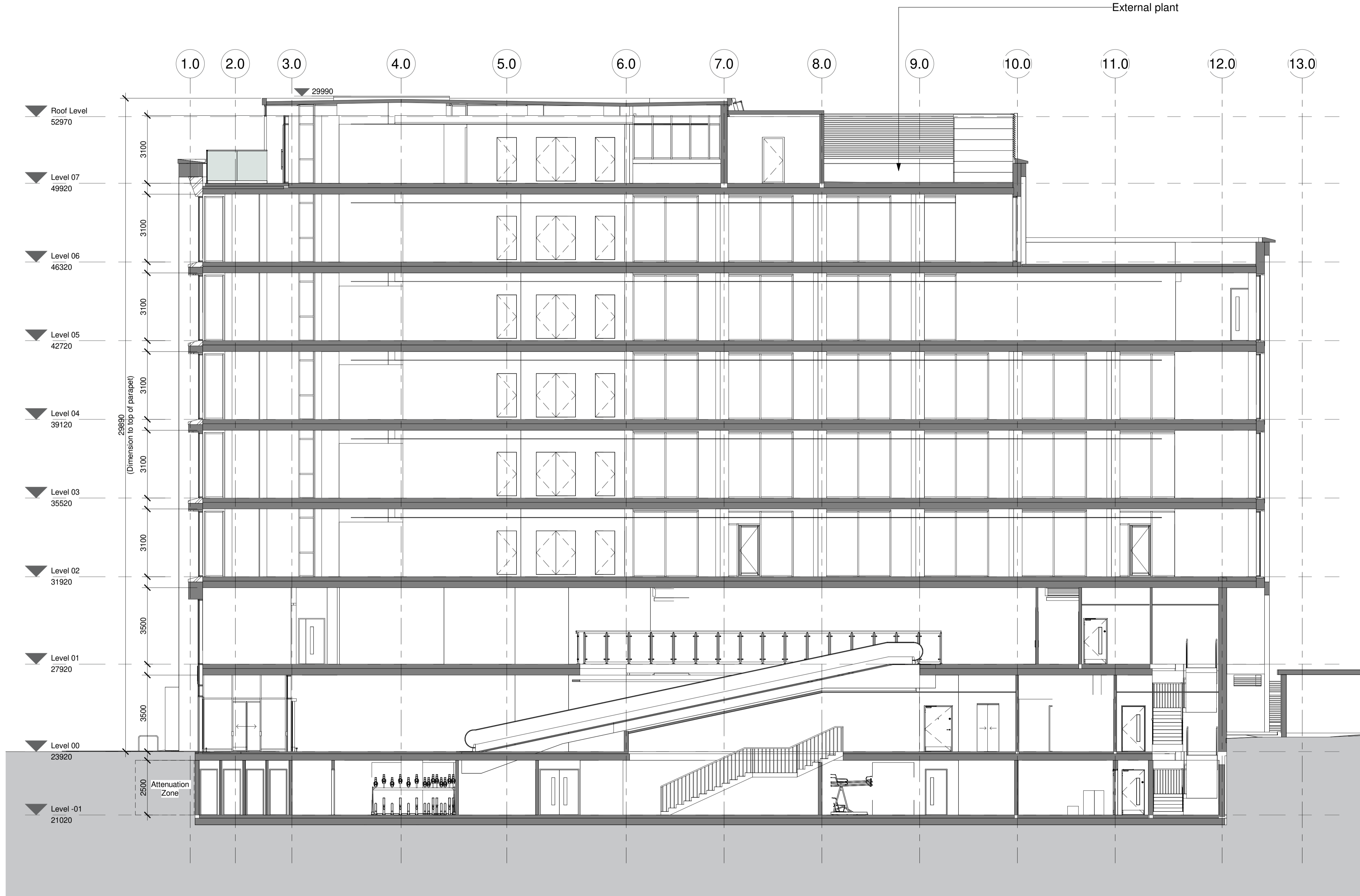
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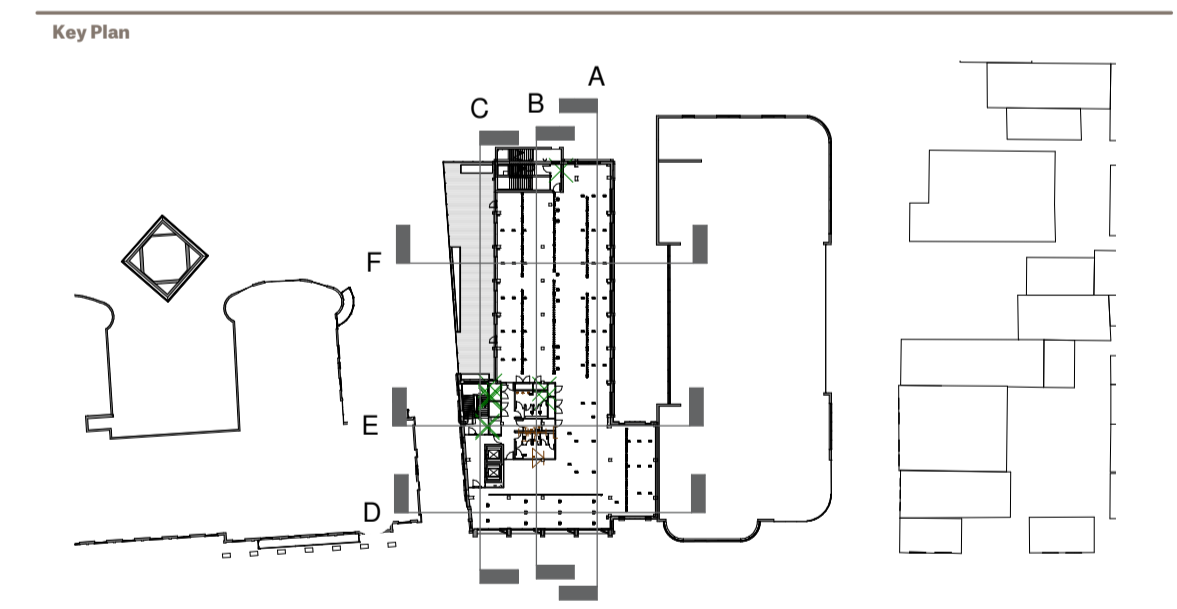
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| Project 07758 - 19 Worple Road | | | | |
| Drawing Title General Arrangement Elevation - Proposed - SW | | | | |
| Project No. 07758 | Drawing No. ZZ-XX-DR-A-02-0002 | Status P9 | Revision 1 | Scale at A1 1:100 |
| File Reference: 07758-SPACE-XX-DR-A-02-0002 | | | | |



| Revision | Description | Author | Checker | Date |
|----------|--|--------|---------|----------|
| P1 | First issue of planning drawings | LC | PB | 29.06.20 |
| P2 | General design development, issued for Board review | LC | PB | 02.07.20 |
| P3 | Design freeze issue | LC | PB | 07.08.20 |
| P4 | Final draft for comment prior to submission | LC | PB | 26.08.20 |
| P5 | General amendments made | LC | PB | 15.09.20 |
| P6 | Draft Planning Submission | LC | PB | 23.09.20 |
| P7 | Planning Submission | LC | PB | 17.11.20 |
| P8 | Planning issue, responding to comments received from the Local Planning Authority. | AW | DW | 06.08.21 |
| P9 | Reissued for Planning | GCI | DW | 19.08.21 |



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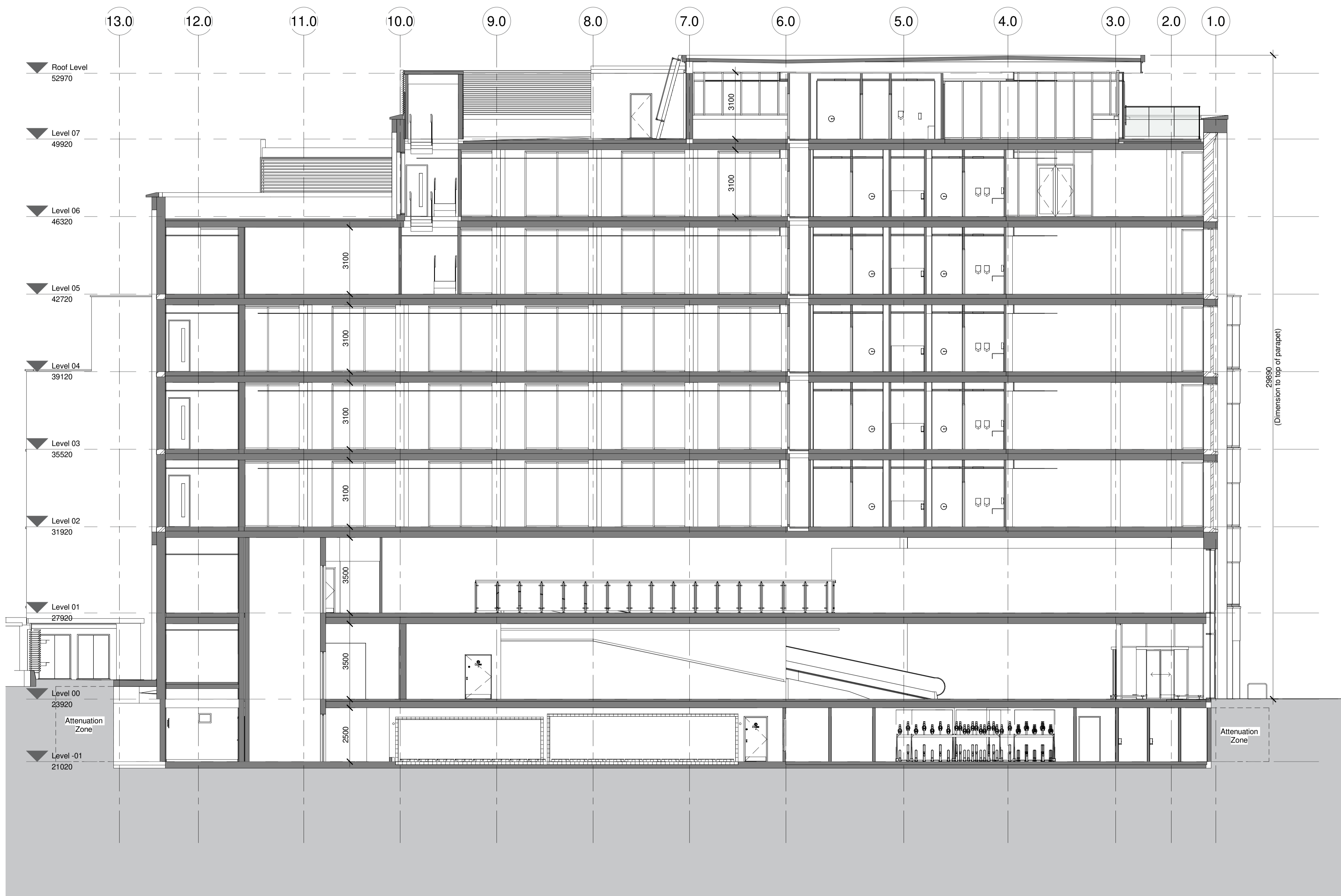
Section A
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| Project 07758 - 19 Worple Road | | | | |
| Drawing Title General Arrangement Sections - Proposed - Sheet 1 | | | | |
| Project No. 07758 | Drawing No. ZZ-XX-DR-A-03-0001 | Status P9 | Revision As indicated | Scale at A1 As indicated |
| File Reference: 07758-SPACE-ZZ-XX-DR-A-03-0001 | | | | |

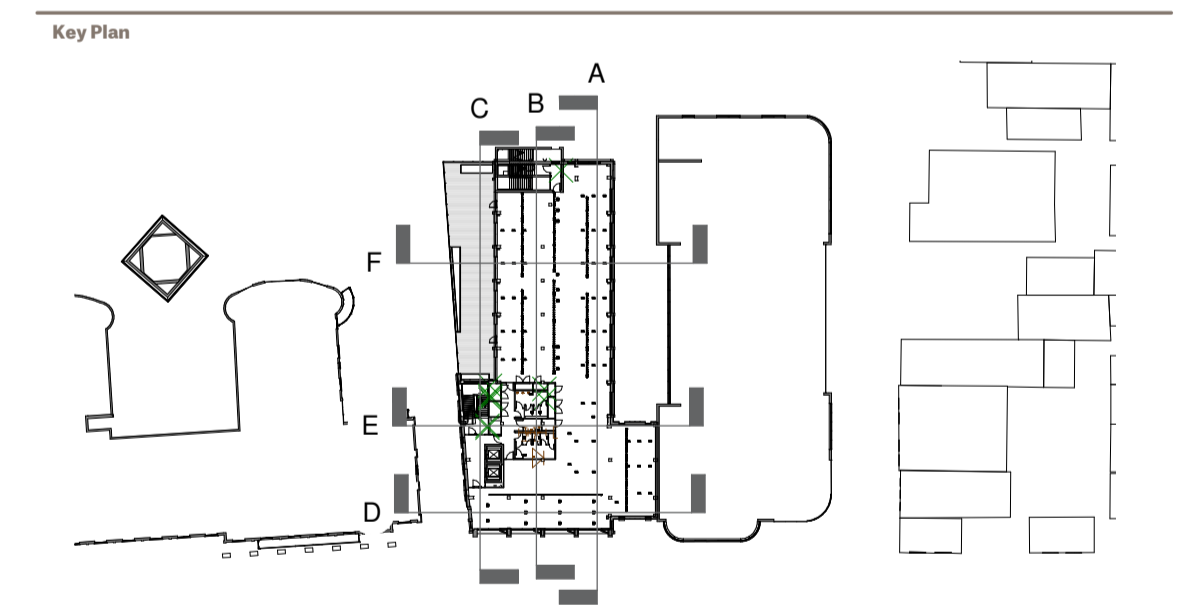


| Revision | Description | Author | Checker | Date |
|----------|--|--------|---------|----------|
| P1 | First issue, issued for Board review | LC | PB | 02.07.20 |
| P2 | Design freeze issue | LC | PB | 07.08.20 |
| P3 | Final draft for comment prior to submission | LC | PB | 26.08.20 |
| P4 | General amendments made | LC | PB | 15.09.20 |
| P5 | Draft Planning Submission | LC | PB | 23.09.20 |
| P6 | Planning Submission | LC | PB | 17.11.20 |
| P7 | Planning issue, responding to comments received from the Local Planning Authority. | AW | DW | 06.08.21 |
| P8 | Reissued for Planning | GCJ | DW | 19.08.21 |

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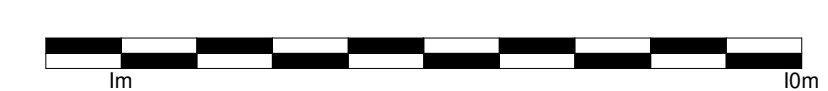
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SPACE

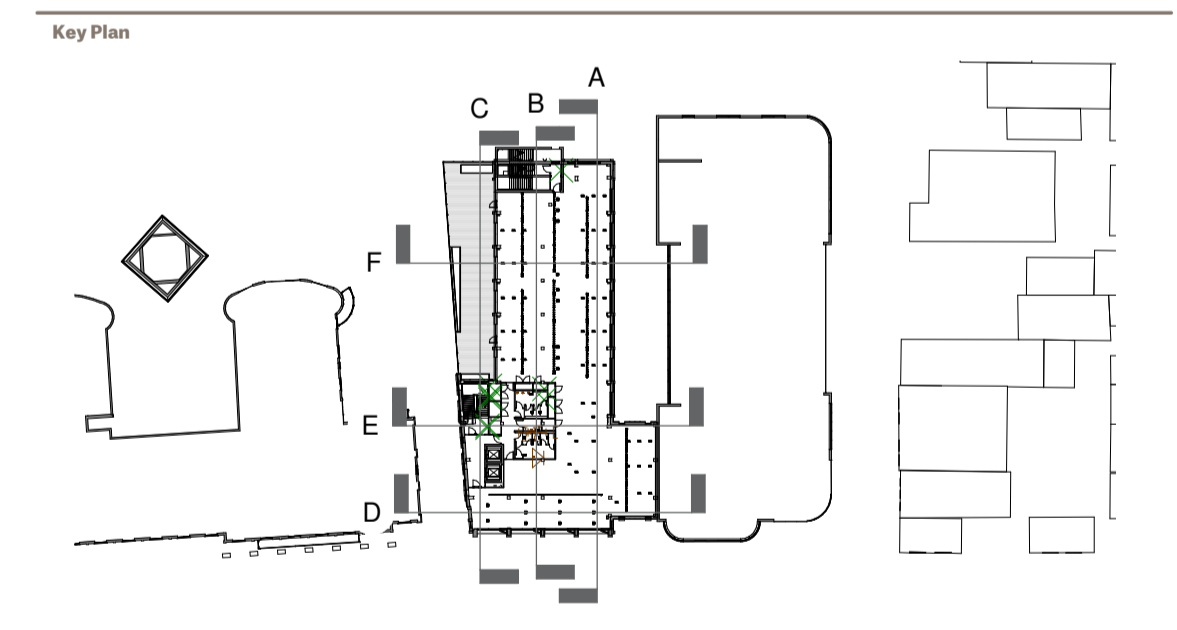
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| Project 07758 - 19 Worple Road | | | | |
| Drawing Title General Arrangement Sections - Proposed - Sheet 2 | | | | |
| Project No. 07758 | Drawing No. ZZ-XX-DR-A-03-0002 | Status P8 | Revision P8 | Scale at A1 As indicated |
| File Reference: 07758-SPACE-ZZ-XX-DR-A-03-0002 | | | | |



| Revision | Description | Author | Checker | Date |
|----------|--|--------|---------|----------|
| P1 | First issue, issued for Board review | LC | PB | 02.07.20 |
| P2 | Design freeze issue | LC | PB | 07.08.20 |
| P3 | Final draft for comment prior to submission | LC | PB | 26.08.20 |
| P4 | General amendments made | LC | PB | 15.09.20 |
| P5 | Draft Planning Submission | LC | PB | 23.09.20 |
| P6 | Planning Submission | LC | PB | 17.11.20 |
| P7 | Planning issue, responding to comments received from the Local Planning Authority. | AW | DW | 06.08.21 |
| P8 | Reissued for Planning | GCJ | DW | 19.08.21 |

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Section C
1 : 100

SPACE

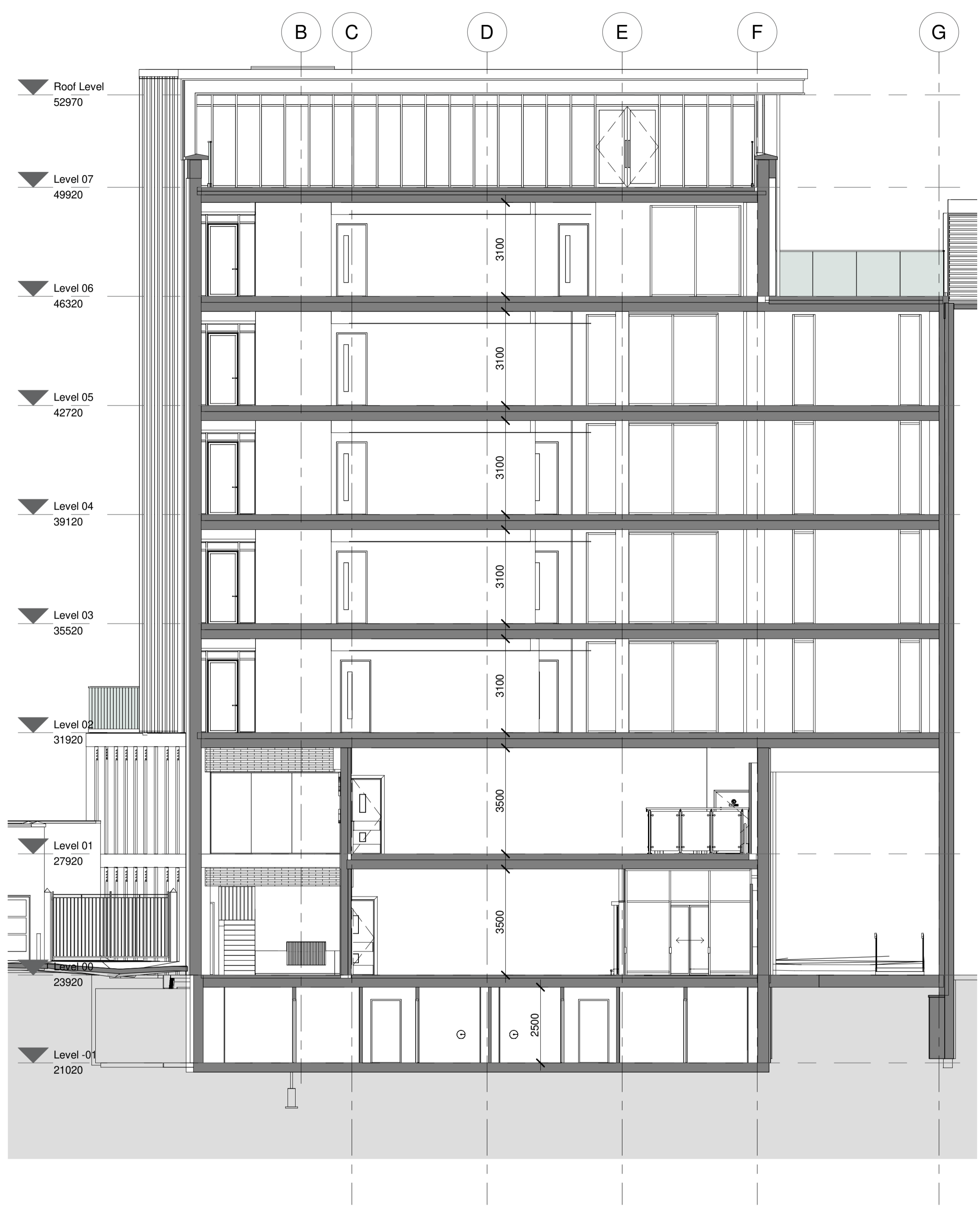
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|---|--|---------------------|-----------------------|------------------------------------|
| Project 07758 - 19 Worple Road | | | | |
| Drawing Title General Arrangement Sections - Proposed - Sheet 3 | | | | |
| Project No. 07758 | Drawing No. ZZ-XX-DR-A-03-0003 | Status P8 | Revision P8 | Scale at A1 As indicated |
| File Reference: 07758-SPACE-ZZ-XX-DR-A-03-0003 | | | | |

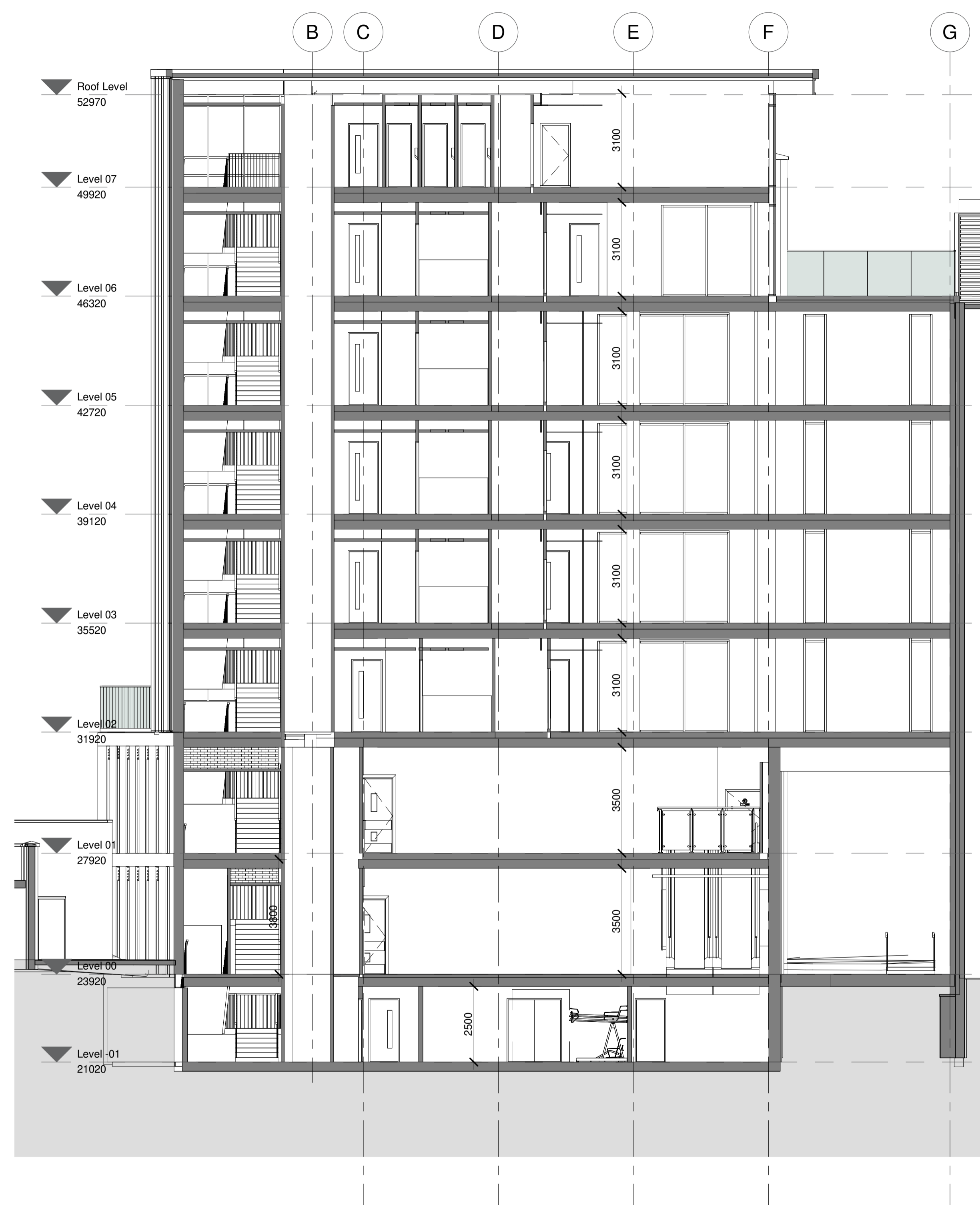


| Revision | Description | Author | Checker | Date |
|----------|--|--------|---------|----------|
| P1 | First issue, issued for Board review | LC | PB | 02.07.20 |
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| P5 | Draft Planning Submission | LC | PB | 23.09.20 |
| P6 | Planning Submission | LC | PB | 17.11.20 |
| P7 | Updated to show extent of Piles | GCI | DW | 17.03.21 |
| P8 | Planning issue, responding to comments received from the Local Planning Authority. | AW | DW | 06.08.21 |
| P9 | Reissued for Planning | GCI | DW | 19.08.21 |

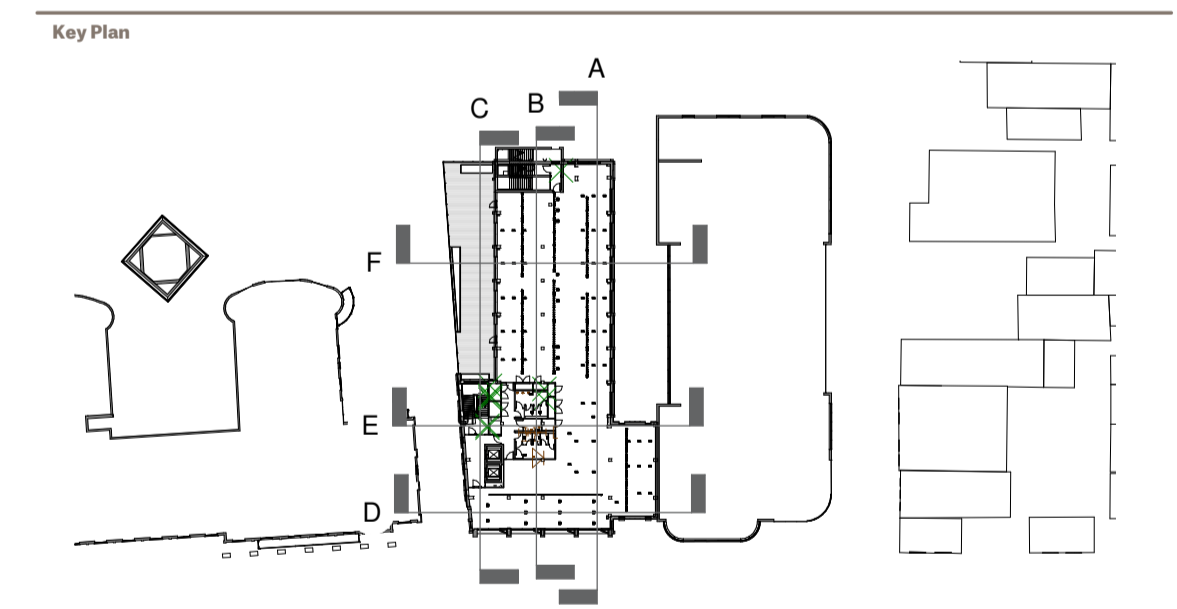
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Section D
1 : 100



Section E
1 : 100



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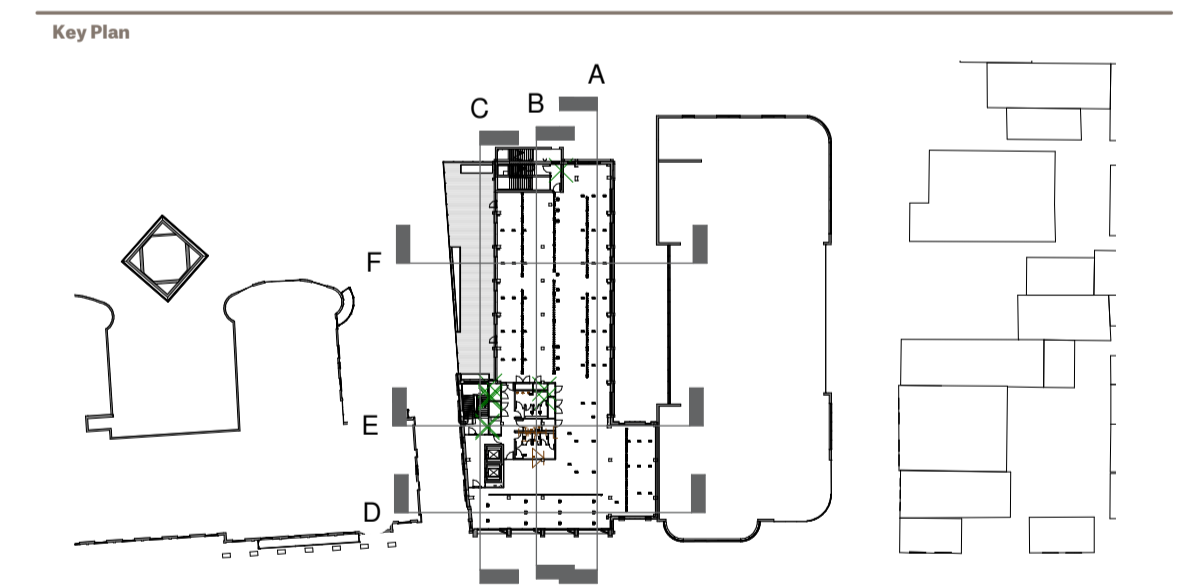
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| Project 07758 - 19 Worple Road | | | | |
| Drawing Title General Arrangement Sections - Proposed - Sheet 4 | | | | |
| Project No. 07758 | Drawing No. ZZ-XX-DR-A-03-0004 | Status P9 | Revision As indicated | Scale at A1 As indicated |
| File Reference: 07758-SPACE-ZZ-XX-DR-A-03-0004 | | | | |



| Revision | Description | Author | Checker | Date |
|----------|--|--------|---------|----------|
| P1 | First issue, issued for Board review | LC | PB | 02.07.20 |
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| P5 | Draft Planning Submission | LC | PB | 23.09.20 |
| P6 | Planning Submission | LC | PB | 17.11.20 |
| P7 | Updated to show extent of Piles | GCI | DW | 17.03.21 |
| P8 | Planning issue, responding to comments received from the Local Planning Authority. | AW | DW | 06.08.21 |
| P9 | Reissued for Planning | GCI | DW | 19.08.21 |



Section F
1 : 100



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| Project 07758 - 19 Worple Road | | | | |
| Drawing Title General Arrangement Sections - Proposed - Sheet 5 | | | | |
| Project No. 07758 | Drawing No. ZZ-XX-DR-A-03-0005 | Status P9 | Revision As indicated | Scale at A1 As indicated |
| File Reference: 07758-SPACE-ZZ-XX-DR-A-03-0005 | | | | |



| Revision | Description | Author | Checker | Date |
|----------|---|--------|---------|----------|
| P1 | First issue of planning drawings | LC | PB | 29.06.20 |
| P2 | General design development, issued for Board review | LC | PB | 02.07.20 |
| P3 | Design freeze issue | LC | PB | 07.08.20 |
| P4 | Final draft for comment prior to submission | LC | PB | 26.08.20 |
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| P6 | Planning Submission | LC | PB | 17.11.20 |
| P7 | Planning issue, responding to comments received from the Local Planning Authority. | AW | DW | 06.08.21 |
| P8 | Reissued for Planning | GCJ | DW | 19.08.21 |
| P9 | Lidl Store curtain glazing frames revised from RAL 5010 to RAL 7024, in accordance with client request. | AW | DW | 08.09.21 |

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- Louvred screening to roof plant
- Seamed zinc cladding with feature slot windows
- Primary Plant
- Brickwork (50mm Danish buff blend)
- Chamfered brick
- Zinc fascia
- Terrace
- Terrace
- Seamed zinc cladding with feature slot windows

- Louvred panels to 'blind' window heads
- Brickwork (50mm Danish grey blend)
- Grade A office entrance
- Retail shopfront curtain wall framing and sealant to be Grey RAL 7024
- Retail entrance
- Bike entrance

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| | | | | |
|---|---------------------------|--------|-----------|-------------|
| Project | | | | |
| 07758 - 19 Worple Road | | | | |
| Drawing Title | | | | |
| 3D Views - Proposed - Sheet 1 | | | | |
| Project No. | Drawing No. | Status | Revision | Scale at A1 |
| 07758 | ZZ-XX-DR-A-04-0001 | | P9 | |
| File Reference: 07758-SPACE-ZZ-XX-DR-A-04-0001 | | | | |

| Revision | Description | Author | Checker | Date |
|----------|--|--------|---------|----------|
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| P7 | Planning issue, responding to comments received from the Local Planning Authority. | AW | DW | 06.08.21 |
| P8 | Reissued for Planning | GCJ | DW | 19.08.21 |



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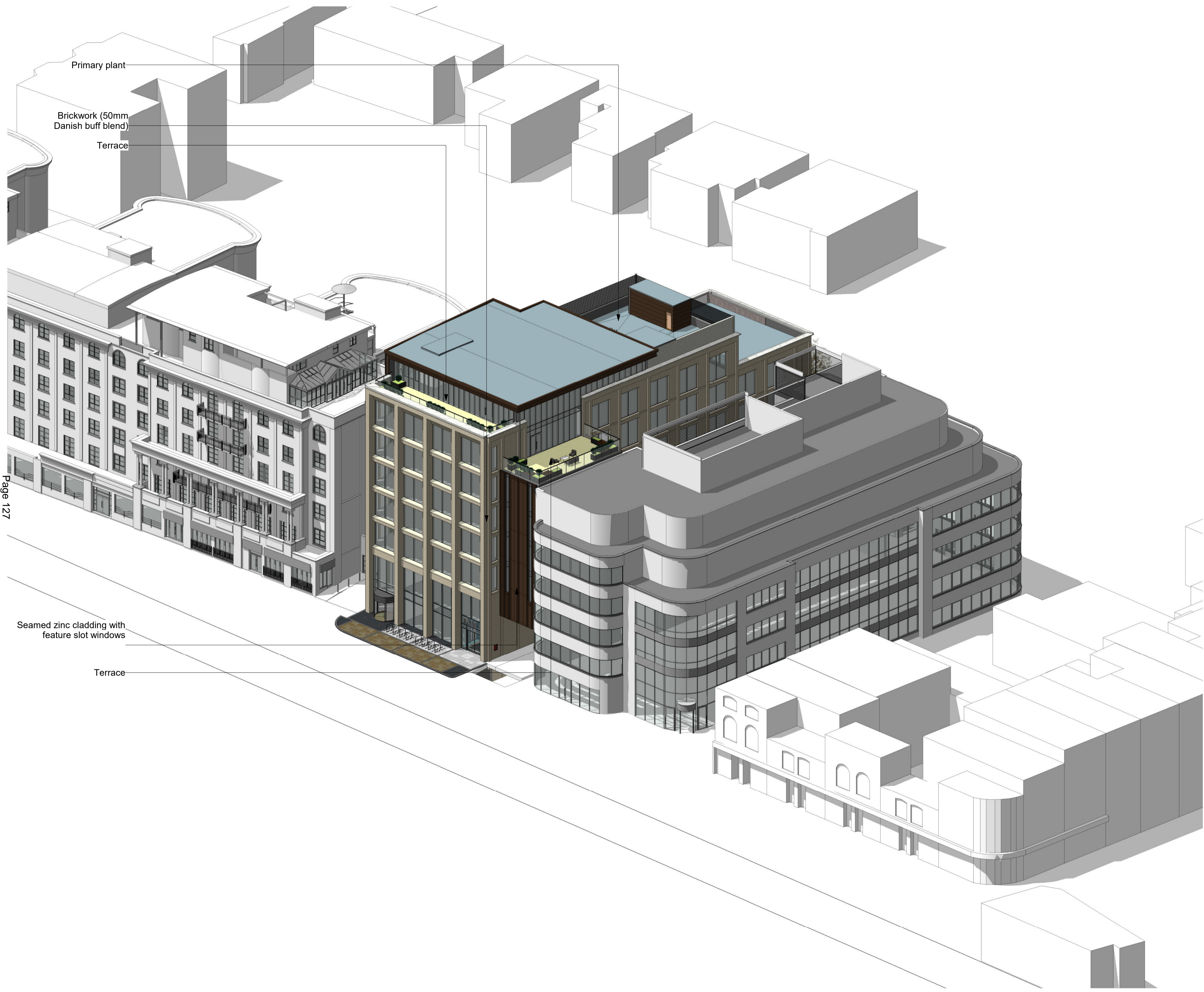
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|---|---------------------------|--------|-----------|-------------|
| Project | | | | |
| 07758 - 19 Worples Road | | | | |
| Drawing Title | | | | |
| 3D Views - Proposed - Sheet 2 | | | | |
| Project No. | Drawing No. | Status | Revision | Scale at A1 |
| 07758 | ZZ-XX-DR-A-04-0002 | | P8 | |
| File Reference: 07758-SPACE-ZZ-XX-DR-A-04-0002 | | | | |

This drawing is Copyright - Check all dimensions on site - Inform originator of any discrepancies - Work to figured dimensions only - Do not scale

| Revision | Description | Author | Checker | Date |
|----------|---|--------|---------|----------|
| P1 | First issue of planning drawings | LC | PB | 29.06.20 |
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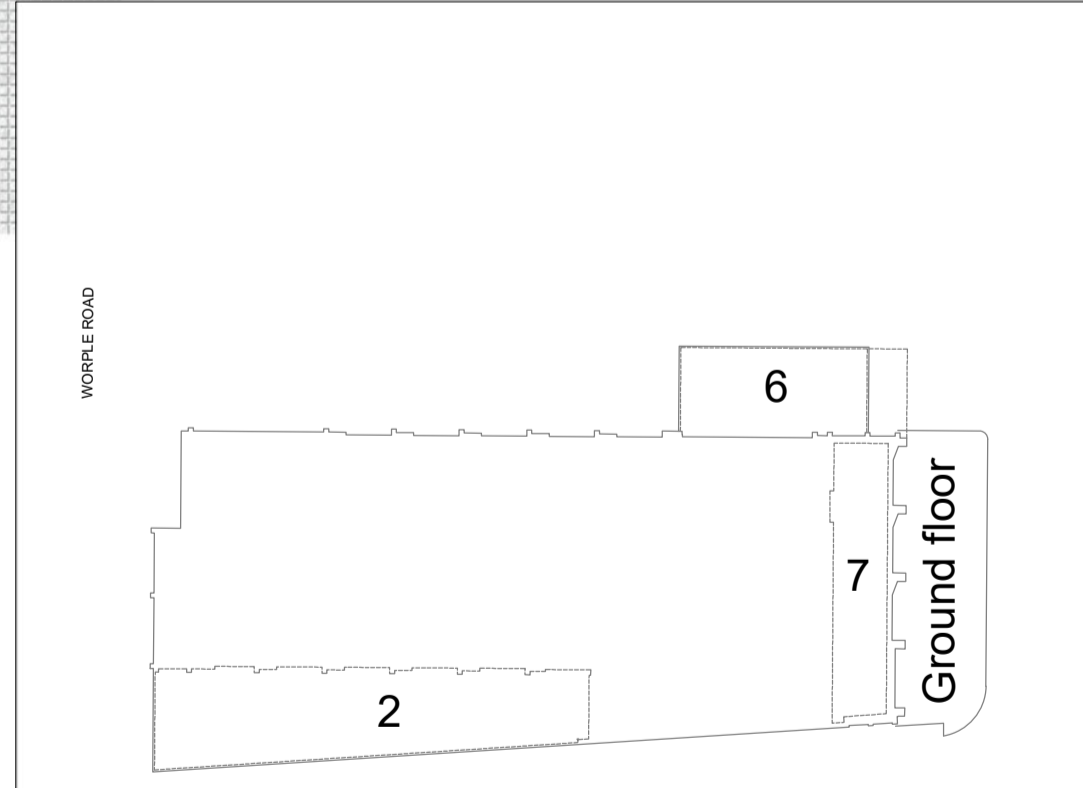


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| | | | | |
|---|---------------------------|--------|-----------|-------------|
| Project | | | | |
| 07758 - 19 Worple Road | | | | |
| Drawing Title | | | | |
| 3D Views - Proposed - Sheet 3 | | | | |
| Project No. | Drawing No. | Status | Revision | Scale at A1 |
| 07758 | ZZ-XX-DR-A-04-0003 | | P9 | |
| File Reference: 07758-SPACE-ZZ-XX-DR-A-04-0003 | | | | |



001 Navigation map - terrace locations
Scale: n/a

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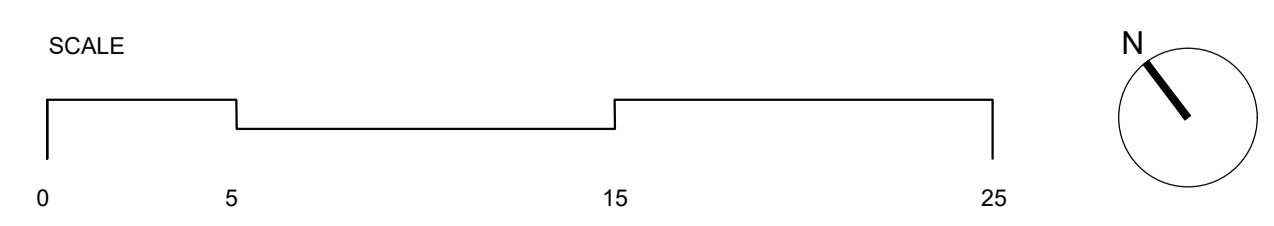
All dimensions to be checked on site

SIGNIFICANT HEALTH, SAFETY & ENVIRONMENTAL INFORMATION RELATING TO CDM

HEALTH:
SAFETY:
ENVIRONMENTAL:

IT IS ASSUMED ALL WORKS WILL BE CARRIED OUT BY A COMPETANT CONTRACTOR WORKING, WHERE APPROPRIATE, TO AN AGREED METHOD STATEMENT

| Rev. | Description | date | by | checked |
|------|--|----------|-----|---------|
| P00 | First Issue | 18/09/20 | MN | KG |
| P01 | Softworks removed from ground floor. Terraces revised. Application boundary amended. | 28/09/20 | MN | KG |
| P02 | Revised design based on planning comments. | 01/09/21 | CHC | KG |



Client
Lidl UK GmbH

Client address
19 Worpole Road
Wimbledon
London
SW19 4JS
United Kingdom

Project Title
LIDL, 19 Worpole Road

Drawing Title
Rendered Landscape Masterplan

Project Stage
Planning

PLACE
Design + Planning

Place Design + Planning Ltd
15 West Barnes Lane
London, SW20 0BU, UK
+44 208944 1940
+44 208944 1117

scale 1:100 at A1 date 18/09/20 drw by MN ch by KG
drw no. 892-PL-002 rev no. P02